

Corporation of the
Village of Cumberland

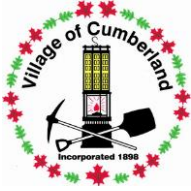
*Heritage
Committee*

Agenda

Monday, August 16, 5:30 p.m.

Council Chambers, 2675 Dunsmuir Avenue, Cumberland, BC

- 1. Welcome to new members**
- 2. Election of Committee Secretary**
- 3. Approval of the Agenda**
- 4. Approval of the Minutes:** May 3, 2021
- 5. Old Business / Business Arising from the Minutes**
None
- 6. New Business**
 - 6.1 Statements of Significance - Waverly Hotel and Cumberland Community Church**
Project start-up and introduction of Elana Zysblat, project consultant
 - 6.2 Council Referral**
Heritage Alteration Permit and Variance Applications – 2714 Dunsmuir Ave
- 7. Next Meeting:** Tuesday, October 7 at 5:30pm.
- 8. Adjournment**



Minutes

Monday, May 3, 2021, 5:30 p.m.
Via video conference*

PRESENT: Meaghan Cursons
Marianne Bell
Hugh McLean
Melissa Roeske
Councillor Gwyn Sproule

STAFF: Karin Albert, Senior Planner

REGRETS: Lena McCliesh

1. Call to Order

2. Approval of the Agenda

Sproule/Bell: "THAT the agenda of May 3, 2021 be approved".

CARRIED

3. Approval of the Minutes: April 6, 2021

Sproule/McLean: "THAT the minutes of April 6, 2021 be approved."

CARRIED

4. Old Business / Business Arising from the Minutes

None.

5. New Business

**5.1 Council Referral - Heritage Alteration Permit and Variance Applications
– 3274 Third Street**

Roeske/McLean

THAT the Heritage Committee receive the "Heritage Alteration Permit and Variance Application – 3274 Third Street (2021-01-HAP)" report.

CARRIED

DISCUSSION:

Variance of front yard setback variance to Penrith:

One variance is to permit a deeper setback from Penrith Avenue. Appropriate since Penrith Avenue is largely residential and the proposed urban bazaar development is directed toward Third Street and the lane to be more connected to the commercial area along Dunsmuir Avenue. The main access to the site will be from Third Street.

Variance of ground floor-to-ceiling height:

The height variance to allow for a lower ground floor-to-ceiling height is appropriate. Ensures the units are less intrusive on the residential neighbour and is a better fit with the feel of an urban bazaar.

Heritage Alteration Permit:

Business spaces will be rentable. Hours of operation would be regulated through Village's business licencing.

Site should interpret/commemorate previous uses, e.g. the livery stable on the TruGreen lot. Also site is directed toward the former Willard building across the lane. Applicant planning to put up a community bulletin board which can include interpretive information. Will look to Heritage Committee to provide leads on information and content.

Applicant and Village to consider how use and bookings of private event space are to be managed to avoid conflict (e.g. sound amplification) with events close-by in public spaces such as Village Square.

Any lighting to be downward facing. Will follow HAP guidelines.

Individual units may have retractable awnings.

Applicant has no plans to fence off site. So far hasn't had problems with site security. Individual units will be locked up. May alarm individual buildings and/or have a closed circuit surveillance system.

Roeske/Sroule:

"THAT the Heritage Committee recommend to Council to approve the development variance (2021-05-DV) to vary Zoning Bylaw No. 1027 to increase the maximum front yard setback from 1m to 8.75m."

CARRIED

"THAT the Heritage Committee recommend to Council to approve the development variance (2021-04-DV) to vary Zoning Bylaw No. 1027 to reduce the minimum ground floor-to-ceiling height from 4.2m to 2.4 m

CARRIED

Roeske/Sroule:

“THAT the Heritage Committee recommend to Council to approve the Heritage Alteration Permit (2021-01-HAP) for 3274 Third Street, substantially in compliance with the permit presented to the Committee at their May 3, 2021 meeting.”

CARRIED

5.2 Recruitment of New Committee members

- Staff is recommending to Council to recruit new members to the Committee. Recruitment message will be broadcast broadly.

6. Next Meeting: Monday, June 7 at 5:30pm.

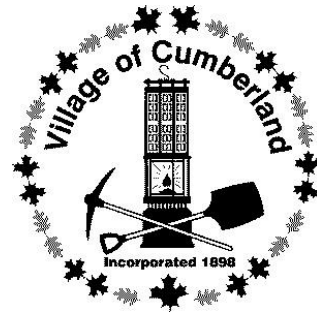
7. Adjournment: 6:40 p.m.

Certified Correct:	Confirmed:
Chair	Deputy Corporate Officer

*This meeting was held through electronic facilities as authorized under Order M192 of the Ministry of Public Safety and Solicitor General during the declaration of a state of emergency made March 18, 2020 in order to conduct business in accordance with public health advisories related to the COVID-19 pandemic. Members of the public who wished to view the meeting were able to email planning@cumberland.ca to receive a link to the on-line meeting.

MEMO

HERITAGE COMMITTEE



REPORT DATE: 8/12/2021
MEETING DATE: 8/16/2021

File No. 3220 – Dunsmuir Ave 2714

TO: Heritage Committee members
FROM: Karin Albert, Senior Planner
SUBJECT: Heritage Alteration Permit and Variance Applications – 2714 Dunsmuir Avenue

RECOMMENDATIONS

- i. THAT the Heritage Committee receive the “Heritage Alteration Permit and Variance Application – 2714 Dunsmuir Avenue (2021-02-HAP)” report.
- ii. THAT the Heritage Committee recommend to Council to request that the applicant revise the design of the proposed building at 2714 Dunsmuir Avenue (2021-02-HAP) to meet the Village’s Heritage Conservation Area guidelines:
 - a. Use brises-soleil, other architectural features, or redesign the façade with the large south-west facing windows in order to reduce heat gain of the corner units in the summer.
 - b. Show a 1.5 metre setback to the lane that is free of any structures.
 - c. Provide a landscape plan (close-up view and plant list) for the planters proposed.
 - d. Provide a close-up of the window frames to be used on the ground floor and for the residential units.
 - e. Show a close-up of the tile features proposed for the entry areas to the commercial units and residential lobby.
 - f. Identify what materials or elements from the old Cumberland Hotel may be used on the interior of the new building.
- iii. THAT the Heritage Committee recommend to Council to request updated photographic renderings of the proposed development at 2714 Dunsmuir Avenue (2021-02-HAP) with views from all four sides, including of the interior courtyard, to be able to better assess how the building’s architecture, materials and colours complement and fit within the streetscape.
- iv. THAT the updated renderings for 2714 Dunsmuir Avenue (2021-02-HAP) be referred back to the Heritage Committee for comment prior to issuing the Heritage Alteration Permit.
- v. THAT the Heritage Committee recommend to Council to deny the development variance (2021-11-DV) to vary Zoning Bylaw No. 1027 to reduce the on-site parking requirement to 14 spaces, therefore requiring cash-in-lieu of parking for spaces that cannot be accommodated on-site.

- vi. THAT the Heritage Committee recommend to Council to approve the development variance (2021-11-DV) to vary Zoning Bylaw No. 1027 to not require any special parking designations on the property, pending a discussion with the developer to provide an electric vehicle plug-in on Second Street fronting the building.

PURPOSE

This memo is supplementary to the report presented to Council on August 9, 2021. The memo provides additional detail on the variance requests and provides an analysis of how the proposed development at 2714 Dunsmuir Avenue meets the Heritage Conservation Area – Historic Village Commercial Core Guidelines in the Village’s Official Community Plan.

BACKGROUND

As per the staff report presented at the August 9, 2021 Council meeting, the applicant is proposing to demolish the Cumberland Hotel and replace it with a four-storey mixed-use development (see Council report).

Bylaw Requirements and Requested Development Variances

Zoning Bylaw Requirements and Requested Variances

The development is located in the VCMU-1 Village Core Commercial Mixed-Use zone. That zone stipulates the permitted uses in the zone, minimum lot size, density, lot coverage, setbacks and maximum building height, as well as conditions of uses and additional regulations (see Attachment 1 – VCMU-1 Zoning information).

The following Zoning Bylaw regulations are particularly relevant to this development application:

- a. The Zoning Bylaw permits four storey buildings in the downtown core. No special permission from Council is required to construct four storeys.
- b. The rear setback to the alley is 4.5 m. Side setbacks are 0 m and the front setback is between 0 m and 1 m.
- c. The parking requirement is 0.9 stalls per residential unit for occupants and 0.1 per residential unit for visitors for a total requirement of 15 parking spaces for the 15 units.
- d. The parking requirement for the commercial spaces depends on the use. Based on the current assumption of uses by the applicant (health clinic, bike store and restaurant), staff calculated that a further 15 parking spaces are required. Final requirements will be determined when the applicant applies for a Building Permit.
- e. The requirement for special parking stall designations are two stalls for parking for persons with a disability, two stalls for parking for pregnant women and persons with young children, two stalls for parking for electric vehicles and one stall for a recreational vehicle.

The table below summarizes the requested Zoning Bylaw variances.

ITEM	REQUIRED	VARIANCE REQUESTED
Parking	30 (estimate)	Reduce to 14
Special Parking Stall Designations	2 persons with a disability 2 pregnant women/persons with young children 2 electric vehicles 1 recreational vehicle	Reduce to 0
Rear Setback	4.5 m	1.5 m – (may be withdrawn by applicant based on turning radius analysis)

The Zoning Bylaw provides the option to the developer to pay cash-in-lieu of on-site parking stalls for up to 30% of the residential stalls and 100% of the commercial stalls. If the final parking requirement is 30 stalls and 14 are provided, then parking cash-in-lieu due will be \$160,000 (16 x \$10,000). That amount is deposited into a municipal cash-in-lieu fund for future parking improvements to made by the Village.

The applicant is currently confirming the feasibility of having electric plug-ins as part of the Klaus multi-park system (which provides the 14 residential parking stalls accessible from the lane). However, their preference is a variance to waive the electric vehicle plug-in requirement.

The Klaus multi-park system does not accommodate the other special parking stall designations.

The applicant will provide a sketch showing the vehicle turning radius into the parking system and confirm if a rear setback is still desired based on that analysis.

Staff recommendation

Staff recommends not to reduce the parking requirement to 14 stalls. That would have the effect of no cash-in-lieu being payable to the Village. The intent of the cash-in-lieu of parking option in the Zoning Bylaw is to allow municipalities to raise funds to make parking improvements to increase capacity and compensate for spaces that are needed but cannot be provided on private property.

Staff recommends to grant the variances to the special parking designations that cannot be accommodated by the Klaus multi-park system. However, staff encourages the owner/developer to enter into discussions with the Village to provide an electric vehicle plug-in associated with one parking stall fronting the building on Second Street.

Heritage Conservation Area Guideline Requirements and Requested Variances

The Heritage Conservation Area Guidelines are part of the Village’s Official Community Plan

Demolishing an existing building and constructing a new building in the Village’s Heritage Conservation Area requires a Heritage Alteration Permit. For the Heritage Alteration Permit to be issued, the applicant has to meet the Heritage Conservation Area guidelines or, ask for a variance to the guidelines.

Following is a review of how the development meets the guidelines.

1. General

Guidelines 1(a) through (c) are not applicable.

The building seeks to conform to the design objectives of the Heritage Conservation Area (HCA-1). Staff is proposing a few changes identified below under the specific guidelines.

The applicant has confirmed that the two lots will be consolidated.

2. Site Design

A parking variance is requested for the visitor parking space associated with the residential units and for the parking for the three commercial units. See discussion above.

A 1.5 m statutory right-of-way to the lane will be required by the Village as part of the development.

The development provides an inner courtyard and a seating area fronting Dunsmuir Avenue.

The draft permit attached to the Council report includes the requirement for a lighting plan to be provided as part of the building permit application.

The development incorporates some natural features by providing planters fronting Dunsmuir and Second Street.

The building has variations in height and massing which helps preserve view corridors to the extent feasible for a four storey building.

Crime Prevention through Environmental Design (CPTED) principles are met.

3. Landscaping

Planter boxes are provided at the entrance to the building, along Second Street and along Dunsmuir. A landscape plan is to be submitted and will form part of the final Heritage Alteration Permit. The plan will outline how the plantings are to be watered.

4. Accessibility

The building provided an elevator which allows for access to the units for persons with physical or mobility impairments. Hard, slip-resistant surfaces are used and the development will comply with width requirements for the BC Building Code. Building and unit accessibility will be reviewed with the Village's Accessibility Committee at an upcoming meeting.

5. Building Form and Character

The private residences on the other side of the lane retain their visual privacy since unit balconies and large windows are oriented toward Second Street and Dunsmuir Avenue.

Fronting Dunsmuir Avenue, part of the building is set back from the main level. The balconies are set into the building also reducing the mass of the building.

The building carries over cornice lines from the adjacent building to the East. The window patterns echoes similar window patterns in other buildings on Dunsmuir Avenue. Windows fronting the street are large and provide interest to pedestrians. There are no solid unbroken walls.

Roof top mechanical equipment, elevator shaft and stairs projecting above the fourth floor are located in the centre of the building and may not be visible from street level. No equipment projects beyond the building.

Building massing is varied adding interest to the building. The setback of the commercial unit fronting Dunsmuir Avenue provides a recessed public seating area and activates the street.

The corner at Dunsmuir Avenue and Second Street intersection is very prominent. The large windows reduce the impact of the building mass. The narrow red-orange coloured multi-storey recess on Dunsmuir Avenue adds interest to that tall wall.

Overall, the building meets the form and character design guidelines. However, the drawings provided make it difficult to interpret materials and colours and how the building fits within the streetscape. Staff recommends that the applicant be asked to provide photographic renderings showing building materials and colours and views from all four sides.

The renderings may be able to address public concerns regarding the prominence of the southwest corner and, generally, how the building fits into the streetscape (see attachments to the Council Report for public comments). The Committee may provide further suggestions to the architect. Attachment 2, a photo record showing some of the existing buildings on Dunsmuir Avenue, is provided to assist with that discussion.

6. Additions and Projections

Not applicable.

7. Sustainable Buildings – Existing

Not applicable.

8. Sustainable Buildings – New

The building allows for natural lighting of the units; however, the large south and west facing windows at the corner of the building mean that those units will heat up considerably in the summer requiring cooling and, therefore, increased energy consumption.

The required bicycle parking for the residential units is provided in the basement of the building.

The development will be required to provide a conduit to the roof for possible future solar power.

Staff recommends that the architect be asked to incorporate brises-soleil / awnings to reduce heat gain in the summer and reduce the building's dependence on energy.

Both sheltered bicycle parking for business owners or employees and public bicycle parking are provided. The architect is asked to show the 1.5 m Statutory Right-of-Way to the lane on the plan. This may affect some of the public bicycle parking and will be reviewed prior to finalizing the Heritage Alteration Permit.

9. Architectural Detailing

The building is a modern interpretation of the historic character of Dunsmuir Avenue. The design does not include fake or imitation elements. Architectural details such as the tall orange coloured recess on the side of the buildings are expressive and bold. Awnings are not provided. As identified above, brises-soleils or recessing of windows is recommended on the Southwest corner to reduce cooling requirements in the summer.

Photographic renderings will make it easier to review architectural detailing.

10. Materials

The project uses modern materials that replicate some of the recommended exterior materials – low profile horizontal drop or bevel siding and wood shingles.

Based on the material samples provided, it appears that the synthetic wood shingles may be a single uniform colour. Variation in colour of the shingles would make the siding visually more interesting.

The applicant states that the entry areas to the commercial units and residential lobby will include tile features. Those are not evident on the drawings and are requested to be provided as part of the revised drawings and renderings.

11. Windows and Doors

The development replicates window patterns used in some of the historic buildings on Dunsmuir Avenue.

Windows are key elements of a building and the architect is asked to provide details and a close-up view for review by Council.

12. Colour

The colour scheme fits within the downtown core.

13. Interior Features

The applicant is asked to identify in what way materials or elements from the old Cumberland Hotel may be used in the interior of the new building.

14. Lighting

As per the guidelines, all lights will be fully shielded and face downward and lighting will not shine into the adjacent residential property. A lighting plan is required as part of the building permit application.

A visual of the interior courtyard is requested to understand how the courtyard will be lit.

15. Signage

The building name sign on Second Street fits the style of the entrance and reminds of older apartment buildings in larger urban centres. It does not seem immediately relevant to the Cumberland context. However, it is respectfully set into the entrance of the building rather than on the wall fronting the street.

Signage of the commercial units is shown to project from the building as is the case on other buildings on Dunsmuir Street. Since commercial tenants have not yet been confirmed, their signs will be reviewed at a later time. The design proposed by the architect will inform that review.

All signs will need to meet the sign regulations in the Village's Zoning Bylaw.

16. Access, Parking and Amenity Areas

Public bicycle parking will be confirmed once the applicant has provided drawings that consider the 1.5 metre setback to the lane. Bicycle parking for the tenants is provided in the basement of the building.

On-site parking uses space efficiently by use of the Klaus multi-park system which stacks cars three levels high.

Building entrances are prominent and clearly visible from Dunsmuir Avenue and Second Street.

The building includes a seating area for the business fronting Dunsmuir Avenue as well as an interior courtyard which can also be used for seating.

17. Stormwater

Stormwater management will include a detention facility to accept the first flush of rain to avoid overloading the Village's stormwater system during heavy rainfall.

18. Soil Erosion and Sediment Control

Soil erosion and sediment control measures are part of the Stormwater Management Plan.

PUBLIC COMMENTS

Public comments received prior to 2 p.m. on August 9, 2021 were provided as part of report to Council. Comments received after that time and date are attached to the end of the Council Report referred to the Heritage Committee.

A number of the comments expressed concerns with respect to the height of the building and fit with the character of the downtown core. Many of the comments were not supportive of a parking variance.

ALTERNATIVES

1. THAT the Heritage Committee recommend that Council approve the Heritage Alteration Permit (2021-02-HAP) substantially in compliance with the draft permit presented to Council at their August 9, 2021 meeting.
2. THAT the Heritage Committee recommend that Council approve the development variance request (2021-11-DV) to vary Zoning Bylaw No. 1027 to reduce the parking requirement for 2714 Dunsmuir Avenue to 14 stalls.
3. THAT the Heritage Committee recommend to Council to deny the development variance request (2021-11-DV) to vary Zoning Bylaw No. 1027 to waive the requirement for special parking designations on the property.

ATTACHMENTS

1. VCMU-1 Zoning Information
2. Photo Review of Some of the Existing Buildings on Dunsmuir Avenue

Respectfully submitted,

Karin Albert
Senior Planner

Attachment 1: VCMU-1 Zoning Regulations

9.4 VCMU-1—Village Core Commercial Mixed-Use Zone

<p>1. Principal Uses</p>	<p><i>Artisan Commercial</i> <i>Auto Service Facility</i> <i>Entertainment Facility</i> <i>Farmer’s Market</i> <i>Hostel</i> <i>Hotel</i> <i>Institutional Use</i> <i>Licensed Premise</i> <i>Market Garden</i> <i>Micro Brewery, Winery, Distillery</i> <i>Mixed Use Building</i> <i>Motel</i> <i>Office</i> <i>Parking Lot/Parking Structure</i> <i>Personal Service</i> <i>Professional Service</i> <i>Restaurant, Major</i> <i>Restaurant, Minor</i> <i>Retail, Garden Nursery</i> <i>Retail Liquor Outlet, Major</i> <i>Retail Liquor Outlet, Minor</i> <i>Retail, Minor</i> <i>Retail, Convenience</i> <i>UBrew/UVin</i> <i>Veterinary Hospital</i> <i>Wholesale</i></p>	
<p>2. Accessory Uses</p>	<p><i>Accessory Buildings and Structures</i> <i>Apartment</i> <i>Bingo Hall</i> <i>Residential Unit</i></p>	
<p>3. Lots Created by Subdivision</p>	<p>Area, minimum</p>	<p>325.0metres² (3,600.0feet²)</p>
	<p>Frontage, minimum</p>	<p>9.0metres (29.7feet)</p>
<p>4. Density</p>	<p>Floor area ratio</p>	<p>2.0</p>
	<p>Building GFA, maximum</p>	<p><i>Accessory buildings</i> shall have a combined floor area no greater than 100.0metres² (1,076.4sfeet²).</p>

9.4 VCMU-1—Village Core Commercial Mixed-Use Zone

6. Lot Coverage	Coverage, maximum		75%
7. Principal Buildings and Structures	Setbacks	Front, minimum	0.0metres (0.0feet)
		Front, maximum	1.0metres (3.3feet) for a minimum of 75% of the <i>building</i> façade
		Rear, minimum	4.5metres (14.8feet)
		Side, minimum	0.0metres (0.0feet)
		Side, Exterior, minimum	0.0metres (0.0feet)
		Side, Exterior, maximum	1.0metres (3.3feet) for a minimum of 75% of the <i>building</i> façade
	Height, maximum	15.0metres (49.3feet)	
8. Accessory Buildings and Structures	Setbacks, minimum	Front	3.0metres (9.8feet)
		Rear	1.5metres (3.9feet)
		Side	1.5metres (3.9feet)
		Side, Exterior	3.0metres (9.8feet)
	Height, maximum	6.0metres (19.8feet)	
9. Conditions of Use	<p>a) Fuel service pumps or pump islands shall be located a minimum of 4.5metres (14.8feet) from any property line.</p> <p>b) A <i>parking lot/parking structure</i> shall have non-Commercial Uses (excluding auto service facility), with a minimum depth of 10.0metres located between the <i>parking lot/parking structure</i> and the front <i>lot line</i> or exterior <i>lot line</i>.</p> <p>c) <i>Commercial uses</i> shall be located to the front of the property facing a <i>highway</i>.</p> <p>e) The principal entrance to a <i>building</i> must face a property line abutting a <i>highway</i>.</p> <p>f) On a <i>lot</i> where there is combined non-residential and <i>residential use</i>, all <i>dwelling units</i> shall:</p> <ul style="list-style-type: none"> i) be contained in the same <i>building</i> that contains the <i>principal use</i>; and ii) have a separate entrance from the exterior, and iii) be located over or to the rear of the building that contains the principal use 		
9. Conditions of Use (con't)	<p>Notwithstanding (iii), residential use on the ground floor to the rear of the commercial units shall only be permitted if all parking requirements can be met on site.</p> <p>g) In the limited circumstances where a use may include a <i>shipping/cargo container</i>, the container must be clad in the same materials as the main</p>		

9.4 VCMU-1—Village Core Commercial Mixed-Use Zone

	<p><i>building</i>, and other measures taken to ensure integration into the site respecting the heritage character of the area.</p>
<p>10. Additional Regulations</p>	<ul style="list-style-type: none"> a) The ground floor of any <i>building</i> shall have a minimum floor-to-ceiling height of 4.2metres (13.8feet). b) <i>Outdoor storage</i> shall not be permitted except for the following uses: <ul style="list-style-type: none"> i) <i>Farmers Market</i>; and ii) <i>Market Garden</i>. c) <i>Shipping/cargo containers</i> are permitted located at the rear of the property: <ul style="list-style-type: none"> i) to a maximum of 6.1metres (20.0feet) in length; and ii) if placed only to supplement commercial storage requirements for <i>commercial uses</i> with limited storage space available within the <i>Principal Building</i>. iii) <i>shipping/cargo containers</i> are not permitted if a <i>residential use</i> is located on the ground level located at the rear of the property. d) <i>Dwelling units</i> are permitted on the ground floor facing the rear <i>lane</i> only, except for an entry lobby providing access to the <i>dwelling units</i>. The entry lobby shall have a <i>frontage</i> less than 25% of the <i>building frontage</i>. e) Where residential and <i>commercial uses</i> are both located on the ground floor, the residential use shall not exceed 40% of the ground floor area. f) <i>Vacation Rental</i> is permitted as a principal use for the properties described below: <ul style="list-style-type: none"> i) That Part of Lot 11, Block 16, District Lot 21, Nelson District, Plan 522A, Lying to the East of a Straight Boundary Joining the Point of Bisection of the northerly and southerly boundaries of said Lot 11 (2771 Dunsmuir Avenue). #1087 ii) The Easterly ½ of Lot 10, Block 16, District Lot 21, Nelson District, Plan 522A (2773 Dunsmuir Avenue). #1087

Attachment 2: Photo Record of Buildings on Dunsmuir Avenue















COUNCIL REPORT



REPORT DATE: 7/23/2021
MEETING DATE: 8/9/2021

File No. 3220 – Dunsmuir Ave 2714

TO: Mayor and Councillors
FROM: Karin Albert, Senior Planner
SUBJECT: Heritage Alteration Permit and Development Variance Permit Applications – 2714 Dunsmuir Ave

FILE	2021-02-HAP and 2021-11 DV		
PID's	002-422-239	FOLIO No.:	516-00059.000
	002-422-255		516-00060.000
LEGAL DESCRIPTION	Lot 1, Block 6, District Lot 21, Nelson District, Plan 522		
	The West ½ of Lot 2, Block 6, District Lot 21, Nelson District, Plan 522		
CIVIC ADDRESSES	2714 Dunsmuir Avenue		
OCP DESIGNATION	Commercial Mixed Use		
ZONE	VCMU-1 Village Core Commercial Mixed use		
VARIANCE REQUEST	REQUIRED	VARIANCE REQUESTED	
	Parking 30	16	
	Rear Setback 4.5 m	1.5 m	
PERMIT AREA	Heritage Conservation Area		

RECOMMENDATION

1. THAT Council receive the “Heritage Alteration Permit and Development Variance Application – 2714 Dunsmuir Avenue” report.
2. THAT Council refer the Heritage Alteration Permit and Development Variance applications for 2714 Dunsmuir Avenue to the Heritage Committee for comment.

PURPOSE

This is an initial staff report to present a Heritage Alteration Permit (HAP) application for a new four storey mixed-use development at 2714 Dunsmuir Avenue, on the current Cumberland Hotel site and to recommend referral to the Heritage Committee. The



development includes three commercial spaces on the ground floor and 15 dwelling units above, a parking garage and outdoor seating area.

The applicant is also applying for a development variance permit to reduce the parking requirement from 30 spaces to 14 and reduce the rear setback from 4.5 m to 1.5 m to accommodate a larger interior patio.

PREVIOUS COUNCIL DECISIONS

There are no previous Council resolutions with respect to this application.

BACKGROUND

The applicant is proposing to demolish the Cumberland Hotel and replace it with a four-storey mixed-use development (Attachment 1 – Design Report). The proposed development has three medium sized commercial units (123 m², 192 m² and 187 m²) on the ground floor and 15 apartment units on floors two to four. Each floor will have two 1-bedroom units and three 2-bedroom units for a total of six 1-bedroom units and nine 2-bedroom units. The building is equipped with an elevator. The entrance to the apartment units opens on to Second Street.

The basement includes secure bicycle storage, bike repair stations, a bike wash station and a dog wash room. The building includes a Klaus multi-parking system for 14 vehicles. The system stacks vehicles in stalls within a semi-automatic parking system to save space. The parking will be accessible from the lane.

One of the commercial units will be accessible from Second Street, one from the corner at Dunsmuir and Second Street and the third unit from Dunsmuir Avenue. The unit that is accessible from Dunsmuir Avenue is intended for restaurant use and includes an outdoor seating area that opens toward Dunsmuir Avenue as well as an atrium/open space at the rear of the unit.



PERSPECTIVE VIEW AT OUTDOOR PATIO SPACE ALONG DUNSMUIR

In addition to the image above, the architect provided renderings for the building and a streetscape (Attachment 1 – Design Report).

Heritage Conservation Area

The OCP designates Cumberland’s downtown commercial core as a heritage conservation area. The historic form and scale of the commercial buildings is integral to the Village’s appearance and ambience. The objective of the Heritage Conservation Area designation in OCP section 11.1.1 is to ensure that “revitalization or new development in the downtown area is compatible with the scale and character of the existing downtown heritage character.”

The Heritage Conservation Area guidelines in OCP section 11.2.1 state: “It is essential to the integrity of a Heritage Conservation Area to have the established heritage character serve as inspiration for new development.”

The applicant provided a Design Report and completed the Heritage Design Guideline checklist to demonstrate how the development complies with the Heritage Conservation Area guidelines (Attachment 1 and Attachment 2). Attachment 3 presents the draft Heritage Alteration Permit. A full analysis of how the proposal meets the Heritage Conservation Area guidelines will be presented in a subsequent staff report with recommendations from the Heritage Committee.

Zoning Bylaw Regulations and Requested Variances

The development proposal complies with the Village’s Zoning Bylaw regulations for the VCMU-1 – Village Core Commercial Mixed Use zone, apart from the proposed variance to the rear (lane) setback. The applicant is applying to vary that setback from 4.5m to 1.5m. If approved by Council, the variance will permit for a larger open patio in the centre of the building, providing for a generous outdoor seating area for the commercial space that is designed for a restaurant. The current drawings show the proposed development without the rear setback variance.

The Zoning Bylaw requires 30 parking spaces on the property: 15 spaces for residential use and 15 spaces for commercial use. The commercial parking space requirement of 15 is based on the assumption of one retail unit, one personal or professional services unit and one restaurant. However, it is possible that only 13 spaces will be required for commercial use because the Zoning Bylaw permits a certain amount of shared spaces between the commercial units. This will be determined at the time of the building permit application. In addition, based on the gross floor area of the commercial spaces, two loading spaces are required.

The Zoning Bylaw provides the applicant with the option to provide cash-in-lieu of parking at \$10,000 per stall for the spaces that cannot be provided on the property. Based on the above estimate of parking requirements, the applicant may be short about 16 spaces and cash-in-lieu payable would be \$160,000.

The applicant is seeking a variance to reduce the residential parking requirement from 15 to 14 and to reduce the commercial parking requirement from approximately 15 to zero. If granted by Council, this would mean no cash-in-lieu is required. Council may grant a variance for the full request of a reduction of 16 spaces, or a reduction of a number fewer than 16 spaces.

Public Notification and Consultation

Pursuant to the Village’s Development Procedures and Fees Bylaw No. 1073, 2018, Council may require applicants applying for a heritage alteration permit to host a neighbourhood public

meeting. The Development Variance Permit requires a neighbourhood public meeting unless exempted by Council.

The applicant is hosting a public information meeting scheduled for Wednesday, August 4 from 4 p.m. to 6 p.m. at the Masonic Hall on Dunsmuir Ave. The meeting was advertised in the Comox Valley Record and the Village mailed the required notice to owners of properties within a 75.0m radius of the proposed development. The applicant has also posted the required public notice sign on the property.

As this report was written prior to the public information meeting, any public comments received prior to the August 9 Council meeting will be provided on table at the meeting. The comments will also be attached to the staff report to the Heritage Committee and will be provided as part of the report to Council that presents the final permits for a decision.

Referrals

Pursuant to Council Policy 1.6, the Heritage Committee advises Council on matters referred to it by Council and reviews and makes recommendations on development applications within any Heritage Conservation Area (HCA) or Heritage Revitalization Development Permit. Staff is recommending referral to the Heritage Committee.

The Heritage Committee can also be asked to comment on the development variance permit application or Council may refer the variances to the Advisory Planning Commission for comment.

ALTERNATIVES

1. THAT Council request that the applicant of “Heritage Alteration Permit– 3274 Third Street (2021-01-HAP)” amend their application to comply with permit guideline(s) _____ by _____ (*please identify requested changes to meet the guidelines*) with the information to be presented as part of the referral to the Heritage Committee.
2. THAT Council not refer the Heritage Alteration Permit(2021-02-HAP) and Variance Applications (2021-11-DV) for 2714 Dunsmuir Avenue to the Heritage Committee for comment at this time but request more information prior to referral.
3. THAT Council refer the Development Variance application – 2714 Dunsmuir Avenue (2021-11-DV) to the Advisory Planning Commission for comment.
4. THAT Council grant the Heritage Alteration Permit (2021-02-HAP) and Development Variance Permit (2021-11-DV) substantially in compliance with the attached draft permits.

STRATEGIC OBJECTIVE

- Quality Infrastructure Planning and Development
- Comprehensive Community Planning
- Healthy Community
- Economic Development

FINANCIAL IMPLICATIONS

None.

OPERATIONAL IMPLICATIONS

Processing of Heritage Alteration Permits and Development Variance Permits are part of the regular services provided by the Development Services Department.

ATTACHMENTS

1. Design Rationale
2. Heritage Alteration Permit Guideline Checklist
3. Draft Heritage Alteration Permit and Schedules
4. Draft Development Variance Permit

CONCURRENCE

Courtney Simpson, Manager of Development Services

Respectfully submitted,

Karin Albert
Senior Planner

Clayton Postings
Chief Administrative Officer

An aerial photograph of a winding road through a dense forest. The road is light-colored and curves from the bottom left towards the top right. The forest is lush green with various tree species. A large, semi-transparent 'DRAFT' watermark is overlaid diagonally across the center of the image. In the top left corner, the text 'THE EDDIE' is written in a large, bold, white sans-serif font, with 'CUMBERLAND, BC' in a smaller, white sans-serif font directly below it.

THE EDDIE
CUMBERLAND, BC

DRAFT

The Eddie

holo-blok + ACI for;
Postmark Development Co.

Cumberland, B.C.
07/23/2021



THE
Eddie
BUILDING



PREFACE

The Eddie is a 4-storey mixed use development located at 2714 Dunsmuir Avenue. The site is zoned Village Commercial Mixed Use (VCMU-1) and is part of the Village's Heritage Management Plan.

This report is designed to tell you the story of how the idea of the Eddie came to be, how the design celebrates the Village's rich identity, and how we intend to support the continued growth of Cumberland while responding to the heritage values.

The proposed project provides 15 residential units of mixed sizes to support a diverse range of residents. The commercial spaces are strategically designed to accommodate small local businesses and enhance the vibrancy of Cumberland's main street.

As we will illustrate in this report, the massing, materials, and urban form were prime considerations that were heavily influenced by Cumberland's past and the vision for the future. The project embodies the authenticity and ruggedness that is synonymous with the Village.

We have loved every moment of designing this project for your Village to date and hope you'll be pleased with the results.



What's in a name?

The Eddie is named after Edward King (not to be confused with King Edward), or "Eddie" as his friends called him. Eddie was from the small town of Lloydminster, Alberta and loved the outdoors. He was an avid sports fan and enjoyed most of his adult life living on his acreage with his wife.

Eddie was authentic, rugged, humble, and honest. Similar to Cumberland, Eddie modesty got his start in the natural resources industry. While he was not a coal miner, Eddie worked his way up to eventually create Kings Energy Group. Eddie was always philanthropic and community minded. To this day, the company he built remains true to his values.



In January of this year, Eddie passed away at the age of 88. He left behind 3 children - one of which was named Darren King. Darren is one of the partners at Postmark Group - the developers of the Eddie. This project could not have been possible without everything Eddie did.

This project is dedicated to Eddie and his memory. It embodies his personality and values. It is authentic, rugged, humble, and honest. It is community oriented, designed for life outdoors, and respectful towards the Village's beginnings.

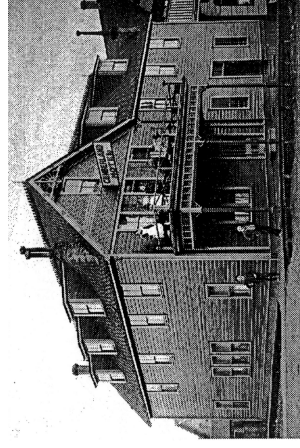
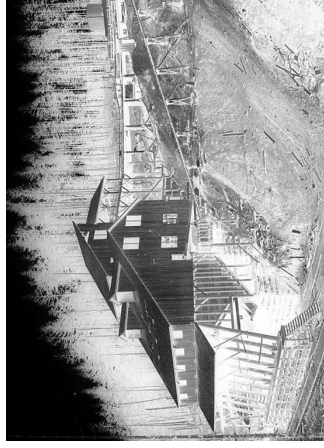
We hope this project will tell his story and we believe that story resonates well with the Village of Cumberland.

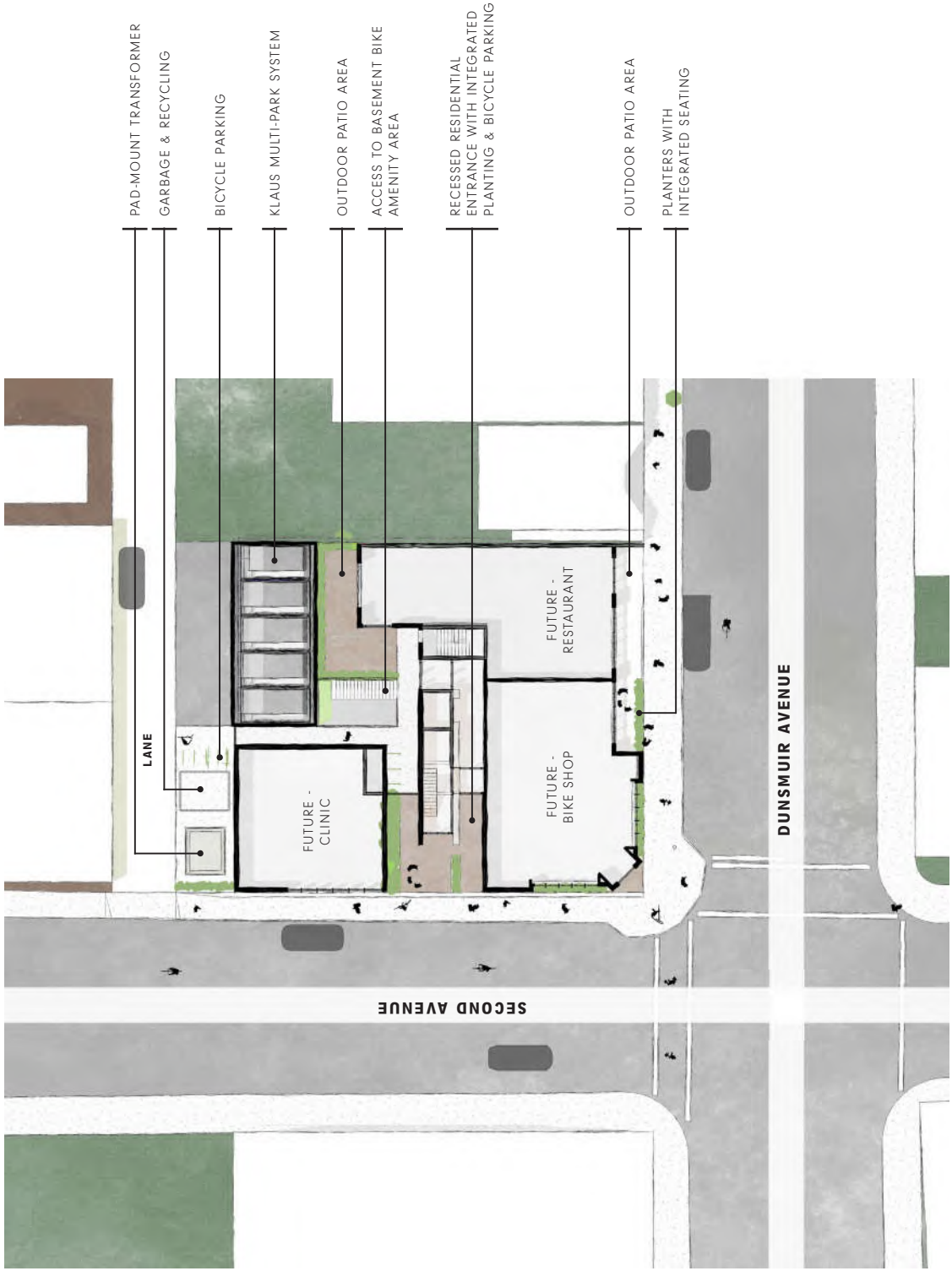


Precedent Study

We started with history. What was Cumberland and how did it come to be what it is today? Through our research, we observed a strong influence from the coal mining industry of the late 1800s. This influence can be characterized by long pitched roof lines, varied massing, timber structures, wood textured finishing materials, and an eclectic mix of colours. Moreover, fenestration is provided in a structured and rhythmic fashion using punched windows with muntin and grille treatments.

This influence is what we've still observed to this date specifically on Dunsmuir Avenue. Our proposal carries forward these ideas as we will demonstrate further throughout this report.





- PAD-MOUNT TRANSFORMER
GARBAGE & RECYCLING
- BICYCLE PARKING
- KLAUS MULTI-PARK SYSTEM
- OUTDOOR PATIO AREA
- ACCESS TO BASEMENT BIKE
AMENITY AREA
- RECESSED RESIDENTIAL
ENTRANCE WITH INTEGRATED
PLANTING & BICYCLE PARKING
- OUTDOOR PATIO AREA
PLANTERS WITH
INTEGRATED SEATING

Site Design

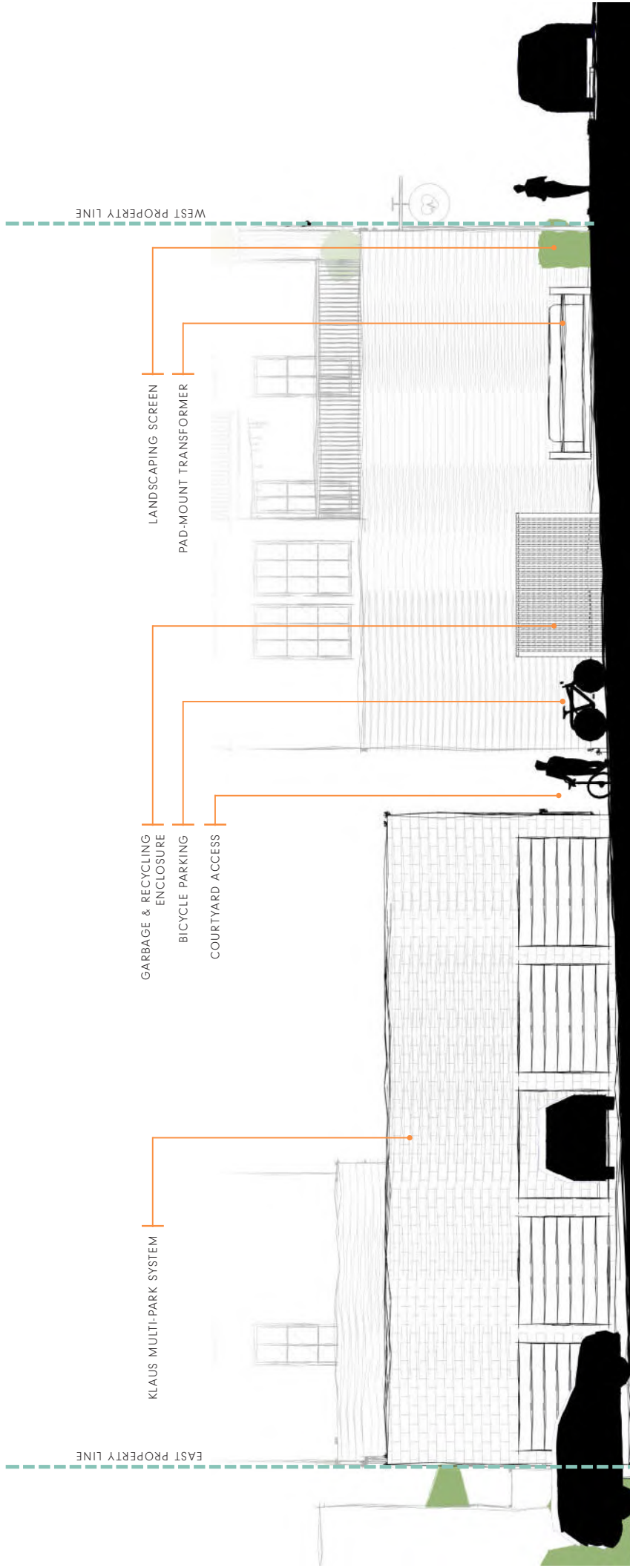
The main driving force behind the site design of the project is to promote a pedestrian friendly, accessible, vibrant, and animated historic downtown village core through the use of transparent storefronts, interior public courtyards, well-lit accessible walkways, and set back facades which activate street level activity. This later feature requires a front setback variance but was a suggestion by the Village that the proposal saw value in incorporating.

- A. OUTDOOR PATIO SPACE
- B. OPERABLE FACADE
- C. KLAUS MULTI-PARK SYSTEM
- D. BICYCLE PARKING
- E. RESIDENTIAL (15 UNITS)
- PROPERTY LINE

Vehicular traffic is diverted from the high-pedestrian areas by accommodating all on-site parking from the rear lane at the rear of the lot. No vehicle access is provided off Dunsmuir. Parking is provided using a multi-park enclosed parking lot system. This system minimizes the footprint and impact of parking stalls while still accommodating the requisite stalls needed to support the residential units. A variance is required for the commercial units but the proposal strikes a balance between the economic development of land within the parameters of the zoning bylaw.



Building Section (East / West)



WEST PROPERTY LINE

LANDSCAPING SCREEN
PAD-MOUNT TRANSFORMER

GARBAGE & RECYCLING ENCLOSURE
BICYCLE PARKING
COURTYARD ACCESS

KLAUS MULTI-PARK SYSTEM

EAST PROPERTY LINE



It was important to the design team to provide an asset back to the community. This is provided through a pedestrian courtyard accessible from 2nd Street. This courtyard serves as an amenity space for a commercial space slated to be a restaurant and also activates the lane side of the development. It is important for the development to engage on all three sides of the property that are not abutting another lot.

The building is oriented on the South and West property lines to maximize access to sunlight. The building is pulled away as much as practical from interior lot lines to avoid impacts to neighbouring properties and to maximize access to light for all.

▲ A recessed residential entrance off Second Street includes planters with integrated seating, electric bicycle parking, and access to the internal courtyard patio space

- A. AMENITY AREA - BIKE REPAIR
- B. ACCESS TO SECURED BIKE STORAGE
- C. AMENITY AREA - EXERCISE ROOM
- D. AMENITY AREA - BIKE WASH
- E. ACCESS TO COURTYARD
- F. OUTDOOR PATIO AREA
- G. ELECTRIC BICYCLE PARKING
- H. RECESSED RESIDENTIAL ENTRANCE
- J. RESIDENTIAL (15 UNITS)
- K. KLAUS MULTI-PARK SYSTEM
- PROPERTY LINE

Sheltered and secure private bicycle parking is provided at the basement level of the development with amenity spaces appropriate to the outdoor playground nature of Cumberland. The bicycle parking will be equipped with charging stations for residents. This works especially well functionally by allowing dirty uses to be separated from clean residential uses.

Public bicycle parking is provided adjacent to the commercial spaces of Dunsmuir Avenue and sheltered under the covered entrance to the residences off 2nd Street.



Building Section (East / West)

Massing & Form

As suggested from the precedent section of our report, the mass and form is heavily influenced by the history of the Village. The main mass at the corner is representative of historic coal mining structures by employing a long single sloped form that is wrapped in a wood shake-looking material. It also provides balance to the compelling sloped roof of The Big Store historic resource across 2nd Street. The main facade facing Dunsmuir then borrows the vertical vernacular found in many historic buildings which employed vertical wood siding.



► Perspective view from the corner of Dunsmuir Ave & Second Street

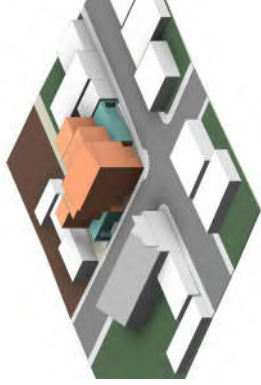
▼ A 4 Storey building mass, with maximum allowable site coverage and floor area ratio.



▼ The building steps away from adjacent properties. The heaviest mass is on the corner of the intersection of Dunsmuir Avenue and 2nd Street.



▼ As the building moves away from this prominent corner, it is set back above the main level to reduce massing and provide enhanced privacy to residents.



▼ The setback also accommodates balconies to enhance natural surveillance, and provides an amenity to residents, while highlighting the mass timber structure being utilized.



South Elevation - Streetscape

The main sloping mass is bookended on the East and North by a flat roof articulation common in modern day Cumberland. This variation in rooflines helps to break down the mass of the proposal so that it can contribute to the varied nature of Cumberland's main street developments. Moreover, the varied single pitch and flat roof solution lends itself to the integration of solar features when appropriate.

CPTED principles are employed through the use of natural surveillance at both the residential and commercial level through plenty of fenestration, transparent vestibules, ample lighting without contributing to glare or shadow, and multiple vantage points to the street level.

Ports of entry are clearly identifiable, well lit, and public facing. Locking gates are used in order to control access at specific times to the public courtyard.



West Elevation - Streetscape



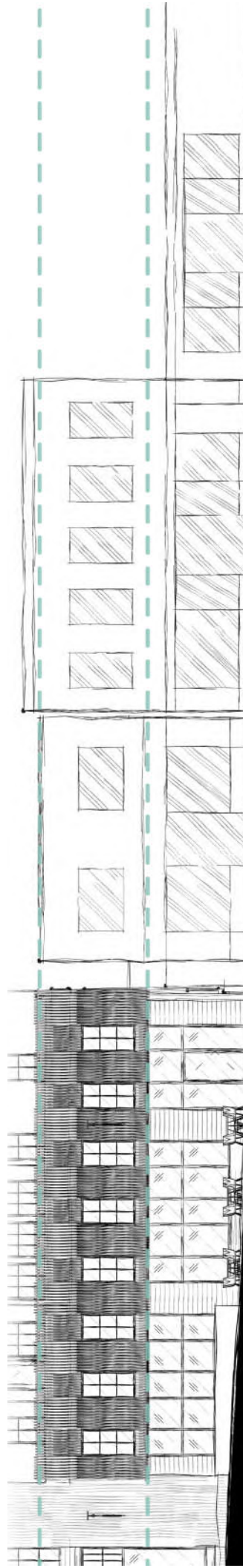
▲ Recessed areas provide outdoor amenity + patio spaces along Dunsmuir Avenue

The building mass respects the importance of the intersection at the corner of the site while breaking down as it moves away from the main intersection. The massing celebrates the corner in the same way the existing hotel had done.

As the massing progresses away from the corner, the massing is brought back down to a pedestrian level, specifically at entrances, to enhance the pedestrian experience at a proportionately respectable scale. Recessed portions of the facade are provided as amenity areas that activate the streetscape.

At the interior lot lines, the proposal lines up with the articulation of adjacent properties. The proposal carries cornice lines of adjacent structures into the massing of the Eddie and also emulates the entrance features of adjacent commercial developments. The proposal integrates repetitive fenestration patterns and utilizes muntin style detailing to respect the historic character of the area.

Large window openings are provided on to the street at the commercial and residential levels. Solid unbroken walls are not present. Rooftop mechanical equipment is hidden by screens that carry the massing expression of the principal building in an unassuming way. The massing is designed so that rooftop access is provided within the overall mass and not as an appendage which is tacked on.



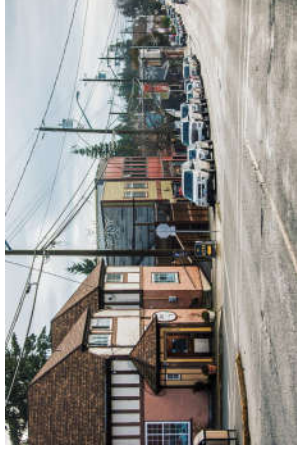
Partial elevation along Dunsmuir Avenue



Perspective view from the corner of Dunsmuir Ave & Second Street

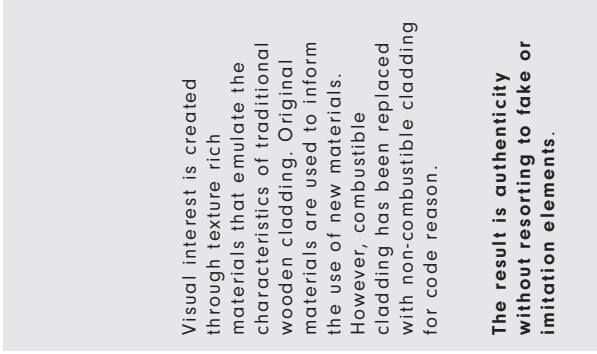
Materials are used that provide visual interest to bypassers using cladding and siding prevalent on dunsmuir and respectful of the historic materials of the area. Building details utilize the historical vernacular and character of the previous dunsmuir hotels but utilize new materials compatible with more stringent code requirements. This re-imagining of the traditional vernacular through the use of new materials provides for a sustainable solution that is respectful to the detailing in the area.

The materials of adjacent structures are used as inspiration for the materials on the new building masses. Horizontal cladding and language is reinforced along the development without disrupting existing views. Moreover, bright colours have been employed in the horizontal wood-like siding to contribute to Cumberland's rich and colourful streetscape.



Visual interest is created through texture rich materials that emulate the characteristics of traditional wooden cladding. Original materials are used to inform the use of new materials. However, combustible cladding has been replaced with non-combustible cladding for code reason.

The result is authenticity without resorting to fake or imitation elements.



▲ B. WOOD LOOK + CHARRED CEMENTITIOUS PANEL



▲ A. WOOD LOOK + SHINGLE CEMENTITIOUS PANEL



▲ D. FIBRE CEMENT PANEL



▲ C. WOOD LOOK + VERTICAL CEMENTITIOUS PANEL



▲ F. TEXTURED CEMENTITIOUS PANEL



▲ E. FIBRE CEMENT PANEL



South Elevation



West Elevation



Village of Cumberland

11.2.2 HERITAGE ALTERATION PERMIT GUIDELINES

The guidelines for this Permit Area are presented in chart format below. Please complete the column 'Application Content' to identify how the development proposal meets each guideline. If a guideline is considered "not applicable" please note N/A and include a brief reason why it doesn't apply.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
1.	General	
a)	Retention of existing siting, roofline design, height, and number of storeys of the affected building/structure. If foundations require replacement, siting and height may be reasonably altered. Whenever possible, original forms, materials and details should be uncovered/left in place and preserved.	N/A – The project is a new build on an existing site.
b)	Alterations shall not interfere with the relationship between the upper and lower storeys.	N/A – The project is a new build on an existing site.
c)	Retail/commercial street level appearance of a building shall not be significantly altered from the original.	N/A – The project is a new build on an existing site. However, the solution pulls the adjacent retail/commercial street level appearances into the new proposed solution.
d)	Adaptive re-use of residential structures on Dunsmuir Ave for commercial purposes is required, provided renovations respect and do not significantly alter the exterior character of the residential building.	N/A – The project is a new build on an existing site replacing an existing hotel.
d)	New construction, especially proposed by chain stores or fast food restaurants, are to conform to the design objectives of the HCA1.	The development is not proposed by a chain store or fast food restaurant. However, the proposal accomplishes the following: <ol style="list-style-type: none"> 1. Recognizes and enhances the historic nature of the Historic Village Commercial Core through the use of materials prevalent in the area and architectural features that complement

Please note that this checklist has been created for convenience. Some of the guideline descriptions have been abbreviated. For the full guidelines, see the Official Community Plan, Bylaw 990.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
		<p>surrounding development. This includes carrying horizontal features from adjacent buildings.</p> <ol style="list-style-type: none"> 2. Respects the history and enhances the heritage character and heritage value of the HVCC through subtle nods to the architectural character of the area through the used of pitched massing and traditional looking materials. 3. Accommodates infill development that is consistent with the existing heritage buildings and enhances the heritage character of the HVCC by maintaining a commercial street presence consistent with Dunsmuir and residential development that uses the vernacular of historic development in the way fenestration and materials are employed. 4. Support densification of the surrounding residential area by providing 15 additional units of various sizes. 5. Promotes a pedestrian friendly, accessible, vibrant, and animated historic downtown Village core through the use of transparent storefronts, interior public courtyards, and well lit accessible walkways.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
e)	Consolidation of properties into large parcels is discouraged unless building design is sensitive to the small scale shop frontages typical of the HCA.	N/A – there is no consolidation occurring with the application.
2.	Site Design	
a)	As the social, cultural, historic, and economic heart of the Village, the HCA requires design consideration which extends beyond the buildings to the site. New construction must meet the parking standard in the Zoning Bylaw. On-site parking is to be at the rear of the lot with access from the lane.	On-site parking is achieved from the rear lane at the rear of the lot using a multi-park enclosed parking lot system that minimizes the footprint and impact of parking stalls. Parking stalls meet the minimum requirements for the residential units but a variance is required for the commercial uses.
b)	A non-buildable 1.5metre area, granted through a Statutory Right-of-Way (SRW), along the rear lane is required for the Village to maintain and provide rear access, travel, and snow removal.	Development is set back from the rear property line with the exception of rear access for garbage collection and the pad mount transformer.
c)	No vehicle access off Dunsmuir Avenue.	The proposal has no vehicle access off Dunsmuir
d)	Where a rear lane present, access shall be from the lane.	Access is provided from the lane.
e)	Site layout to consider locating buildings to incorporate pedestrian courtyards, plazas, and common gathering areas with coordinated site furniture and lighting.	A pedestrian courtyard is provided off the street with access to a commercial unit slated to be a restaurant. The courtyard will contain site furniture and lighting. Please refer to the renderings.
f)	Site design shall consider the preservation or enhancement of natural site features.	There are no natural site features to preserve. However, the proposal responds to the natural grade of the site and accommodates accessibility along the existing sidewalk to all commercial units and the residential development.
g)	Site design shall maintain key view corridors to open areas and surrounding landscape vistas.	The proposal maintains existing view corridors to adjacent open areas within the parameters of the zone. The building is set back as much as practical from the adjacent

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
		neighbour to minimize impact while maximizing allowable density.
h)	Site design and landscaping shall demonstrate the principles of Crime Prevention through Environmental Design (CPTED).	<p>CPTED principles are employed through the use of natural surveillance at both the residential and commercial level through plenty of fenestration, transparent vestibules, ample lighting without contributing to glare or shadow, and multiple vantage points to the street level.</p> <p>Ports of entry are clearly identifiable, well lit, and public facing. Locking gates are used in order to control access at specific times to the public courtyard.</p>
3.	Landscaping	
a)	Mature plantings that provide historic context and character-defining elements, shall be taken into consideration in site redevelopment and new construction.	There is an absence of mature plantings on the site that can be retained. However, a public plaza will integrate new plantings to further enhance the pedestrian experience.
b)	To maintain the existing open appearance, the height of fences or solid hedges between the front of the principal building and the front lot line shall be limited.	No fences or solid hedges are proposed.
c)	Where construction of a new fence is contemplated, the fence or wall is encourage to be of historic appearance (e.g., various styles of pickets or stone walls)	No fences are contemplated.
d)	Landscaped areas shall be watered by an automatic irrigation system, complete with an automated 'smart' controller.	No irrigation system is required or proposed.
e)	Except where precedent exists for renovation of historic landscapes or gardens, high water use turf, sod, and lawn are prohibited.	No high water use turf, sod or lawn are proposed.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
f)	Landscape planting shall minimize water consumption through use of native and drought-resistant plants. Rainfall capture systems shall be incorporated for irrigation where feasible.	Where plantings are proposed, native and drought resistant plants will be employed at.
4.	Accessibility	
a)	All new construction requires installation of sidewalks along the frontage and, if on a corner lot, along the adjacent side street to the rear lane.	The character of the existing sidewalks is respected and access is provided from those sidewalk elevations into each access point.
b)	Accessibility features shall be integrated into the design and identified on the site plans.	No additional accessibility features are required given the zero lot line development.
c)	Accessible travel routes shall be provided from adjacent roadways and parking areas to the main building entry and shall incorporate barrier-free universal design principles.	The main entrance to the residential lobby and each commercial unit is provided from the adjacent roadway and enclosed at grade parking lot utilizing barrier-free principles.
d)	Accessible travel routes shall be of a hard, slip-resistant surface with a minimum width per requirements of the most current edition of the British Columbia Building Code (BCBC).	Accessible routes provided on site are a hard, slip-resistant surface that comply with minimum width requirements in the BC Building Code. All other routes are public domain sidewalks.
e)	Accessible travel routes shall conform to the requirements of the most current edition of the BCBC.	All travel routes provided on site conform to the requirements of the BC Building Code.
5.	Building Form and Character	
a)	Buildings shall be sited so that any adjacent residential properties have visual privacy, as well as protection from site illumination and noise.	There are no adjacent residential properties in the vicinity. However, the main fenestration is outward facing from units and is directed to Dunsmuir and the adjacent street.
b)	New buildings in the area shall give special consideration to the following:	
i)	New buildings shall not be set back from the street or side property lines unless there are specific design reasons, such as the development of a garden for seating, built into the design. Parking is not permitted in	The building is set back above the main level to reduce massing and provide enhanced privacy to residents. The setback also accommodates balconies to

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
	front of a building that is set back from the street;	enhance natural surveillance and to provide an amenity to residents, while highlighting the mass timber structure being utilized.
	ii) New buildings shall respect the design of adjacent buildings. For example, new construction can carry through a cornice line in the new structure or repeat the window pattern of an adjacent building;	The proposal carries cornice lines of adjacent structures into the massing and also emulates the entrance features of adjacent commercial developments. The proposal integrates repetitive fenestration patterns and utilizes muntin style detailing to respect the historic character of the area.
	iii) All new buildings shall have large window openings onto the street. New buildings shall not be constructed with solid unbroken walls facing the street which appear uninteresting and uninviting to pedestrians.	Large window openings are provided on to the street at the commercial and residential levels. Solid unbroken walls are not present.
c)	All buildings and expansions thereto, storage, parking, and supply yards shall be designed to be compatible with surrounding land uses and the major roads fronting the property.	All storage and parking are provided in a compatible manner at the rear of the site to preserve the pedestrian character of Dunsmuir.
d)	Rooftop mechanical equipment shall be hidden behind screens or parapets designed as an integral part of a building to conceal such equipment.	Rooftop mechanical equipment is hidden by screens that carry the massing expression of the principal building in an unassuming way. The massing is designed so that rooftop access is provided within the overall mass and not as an appendage which is tacked on.
e)	Building massing shall respond to a human scale with materials and details that are proportionate to human height and provide visual interest at the street and sidewalk level.	Building massing respects the importance of the intersection at the corner of the site while breaking down as it moves away from the main intersection. The massing celebrates the corner in the same way the existing hotel had done. As the massing progresses away from the corner, the massing is brought back down to a pedestrian level, specifically at entrances, to enhance the

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
		pedestrian experience at a proportionately respectable scale. Materials are used that provide visual interest to bypassers using cladding and siding prevalent on Dunsmuir and respectful of the historic materials of the area.
f)	The form and scale of new construction and materials shall respect and complement the adjacent structures. Building massing shall consider the preservation or enhancement of view corridors to open areas and mountain vistas.	The materials of adjacent structures is used as inspiration for the materials on the new building masses. Horizontal cladding and language is reinforced along the development without disrupting existing views. The massing is setback from the street level above the commercial level to allow for light penetration and to open up view corridors above street level.
g)	The original proportions of storefront elements (i.e., windows, doors, entranceways, cornices, and bulkheads) shall be retained or if necessary reconstructed.	Building features are not being re-constructed. However, the tudor style of the existing hotel is being re-imagined using new materials and retaining existing proportions of the details.
6.	Additions and Projections	
a)	Additions to heritage buildings shall conform to the massing of the existing structure. This is a core principle in maintaining heritage character.	N/A – the building is a new proposal and not an addition to a heritage building.

Heritage Alteration Permit Requirements		Development Proposal	
11.2.2	HCA#1 Heritage Village Commercial Core		
	i)	Any new construction shall blend sensitively where it joins with an older building;	N/A – the building is a new proposal and not an addition to a heritage building.
	ii)	The visual impact of building additions shall be minimized from adjoining streets;	N/A – the building is a new proposal and not an addition to a heritage building.
	iii)	Additions to the front of historically intact heritage building façades may not be permitted, except where the proposed addition replaces an existing addition or where the addition is a porch;	N/A – the building is a new proposal and not an addition to a heritage building.
	iv)	Architectural elements such as front porches, verandahs and bay windows shall be retained and, where possible, restored to their original design.	N/A – the building is a new proposal and not an addition to a heritage building.
7.	Sustainable Building - Existing Buildings within the Heritage Conservation Area		
	a)	Materials: Retain existing building envelope materials as possible, including siding. Design and install rain screen sidings with care, as they have the potential to introduce life cycle considerations and may impair heritage character through the removal of original material.	N/A – the building is not an existing building.
	b)	Windows and Doors: For historic buildings, every reasonable attempt shall be made to repair original window sashes and doors, or to replace inappropriate later additions with replicas of the originals. Thermal efficiency may be achieved through the repair and maintenance of existing wooden windows. Wood-framed storm windows will also aid with thermal efficiency and sound abatement. Replacement of originals windows shall only be undertaken as a final resort in cases of extreme deterioration.	N/A – the building is not an existing building.
	c)	Mechanical Systems: Inefficient mechanical systems are one of the main reasons why existing buildings are poor thermal performers. Consider installing energy-efficient appliances when possible.	N/A – the building is not an existing building.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
d)	Insulation: Introduce extra insulation, especially in attic spaces. Consider the use of weather-stripping and other draft- proofing measures.	N/A – the building is not an existing building.
e)	Improve the performance of heritage and existing buildings, including by reference to the Vancouver Heritage Foundation’s Old Buildings: <u>Your Green Guide to Heritage Conservation</u> available at <u>www.vancouverheritagefoundation.org</u>	N/A – the building is not an existing building.
8.	Sustainable Building - New Buildings within the Heritage Conservation Area	
a)	Building design should include passive heating, lighting and cooling design features.	The building utilizes operable windows and provides plenty of fresh air to residential suites to augment passive heating and cooling. Substantial fenestration is provided, particularly on the South and West facades, which provide ample natural lighting.
b)	Landscaping and building design should consider the incorporation of natural daylight and seasonal shade needs.	Cantilevered balconies help to shade fenestration in the summer months and minimize heat gain while still maximizing access to daylight in all seasons through the provision of significant fenestration.
c)	Building orientation should, where practical, be designed to optimize the benefits of solar orientation.	The building is oriented on the south and west property lines to maximize access to sunlight. The building is pulled away as much as practical from interior lot lines to avoid impacts to neighbouring properties and to maximize access to light for all.
d)	Building design should incorporate solar ready features.	The building design possesses flat roofs which lend themselves to the integration of solar features. Moreover, the primary pitch on the site is directed west which would see the greatest amount of afternoon intense sunlight.
e)	Sheltered and secure bicycle parking shall be provided to a level that is consistent with the proposed use.	Sheltered and secure bicycle parking is provided at the basement level of the development with

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
		amenity spaces appropriate to the use. Access is provided from the exterior to separate the interface between clean and possibly dirty uses.
f)	Electric vehicle plug-ins should be provided for new developments.	Electrical plug-ins are not possible using the parking solution proposed. The parking solution was chosen to minimize the at grade impacts of a large parking lot and the sustainable impact of this solution is much better than conventional parking solutions.
g)	All buildings shall be designed and engineered to be solar ready.	The building design possesses additional roof capacity for solar ready features. The electrical systems are designed to integrate this technology if and when desired. The structural design will allow a reserve 0.5kPa (10psf) design load on all unoccupied roofs, for provision of future solar panel arrays.
h)	Adaptive reuse of buildings and on-site materials is encouraged, where permitted within the BC Building Code and under relevant legislation.	Existing materials are combustible in nature and un-salvageable in their current state.
9.	Architectural Detailing	
a)	Design proposals for heritage buildings shall be examined to determine what original architectural details remain and may be rehabilitated. The historic character of heritage buildings is dependent on a variety of architectural details. It is recognized that:	

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
	i)	In some cases, certain features have been lost or obscured by many years of weathering, inappropriate renovation or lack of maintenance;
	ii)	Not every detail of every building may be feasibly restored, but surviving features shall be retained and repaired. Inappropriate later additions shall be removed or replaced.
	b)	Inappropriate new architectural details will not be permitted.
	c)	Building details shall be compatible with the date the building was constructed or, where appropriate, a historically defensible later dates, and shall be based on documentary evidence.
	d)	Fake or imitation "heritage looking" elements such as sunburst windows or "gingerbread" woodwork shall not be applied to the façades of buildings if they were not a part of the original building.
	e)	Awnings are recommended, where appropriate. The awning or canopy shall be well set back from the curb.
		The existing structure was in a state of disrepair, weathering, and are inappropriate for refurbishment.
		The existing tudor style of the Cumberland hotel is ironically out of context with the rest of Dunsmuir. However, the proposal reimagines those tudor features and draws inspiration from the original hotel on the site which was much more in line with the other developments on Dunsmuir.
		Inappropriate and new architecture details are not proposed. The details are consistent with adjacent developments and are inspired by the historical character of the area.
		Building details utilize the historical vernacular and character of the previous Dunsmuir hotels but utilize new materials compatible with more stringent code requirements. This re-imagination of the traditional vernacular through the use of new materials provides for a sustainable solution that is respectful to the detailing in the area.
		Fake and imitation elements were avoided. However, the elements were re-interpreted using new materials so that the development is a genuine representation of the time it was built with sincere nods to the character of the area.
		Covered entries are provided at the principal entrances to commercial units and to the residential lobby. Awnings can be provided if requested by the village with the

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
		provision of an encroachment agreement over the sidewalk.
f)	Awning and canopy dimensions shall be as follows:	
i)	2.8metres minimum height from ground level to bottom of awning;	Awnings can be provided if requested by the village with the provision of an encroachment agreement over the sidewalk at this dimension.
ii)	1.8metres maximum projection from building;	Awnings can be provided if requested by the village with the provision of an encroachment agreement over the sidewalk at this dimension.
iii)	45degree awning or canopy angle;	Awnings can be provided if requested by the village with the provision of an encroachment agreement over the sidewalk at this dimension.
iv)	1.8metres maximum frame height.	Awnings can be provided if requested by the village with the provision of an encroachment agreement over the sidewalk at this dimension.
10.	Materials	
a)	Original materials shall be maintained in order to ensure visual continuity. Any new materials used shall respect both the style and the date of the individual building.	Original materials are used to inform the use of new materials. Combustible cladding has been replaced with non-combustible cladding for code reason. However, the look of the materials is respectful to the style of existing buildings in the area.
b)	Original materials shall be left in place, or exposed when intact.	Original materials are not incorporated in the proposal at this time. When demolition occurs, the team will look to preserve any features for integration into the interior design where code restrictions and performance considerations are not impacted.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
c)	All materials used in alterations or additions shall be sympathetic in appearance to the original structure.	N/A the building is not an alteration or addition.
d)	Original wood siding and trim shall be repaired, painted and maintained to a generally acceptable standard. This is both a sound restoration and environmental practice. If replacement is necessary. In these cases, the original configuration, assembly and appearance of wooden elements shall be duplicated.	N/A the existing structure is stucco and cannot be preserved. Wooden-looking cladding is used in a non-combustible application for code reasons.
e)	Recommended Exterior Materials:	
i)	Low profile horizontal drop or bevel siding, where appropriate	Non-combustible low profile horizontal siding is used on the residential floors.
ii)	Wood shingles, where appropriate	Non-combustible wood shingle-looking cladding is used to wrap the main massing element.
iii)	Sandstone and good quality brick, where appropriate	Masonry materials are not contemplated for the project at this time.
iv)	Appropriately designed stucco as a cladding material, where appropriate	Stucco is not being proposed because the envelope is using a true rainscreen PERSSIST system.
v)	The use of tile as a decorative entryway and façade material is encouraged. Tile shall be limited to small decorative areas. Tiles shall be relatively small, 152.0millimetres or less square, and solid in colour	The entry areas in front of the commercial units and the residential lobby will tile features to enhance those portals.
11.	Windows and Doors	
a)	Several of the early Cumberland buildings originally had double-hung or casement wooden sash windows and wooden doors. Every attempt shall be made to repair original windows or to replace inappropriate later additions with replicas of the originals).	Operable double hung windows are contemplated for the residential portion of the building.
b)	Metal Frame Windows and Curtain Walls are acceptable if there is clear demonstration of scale, material, colour and pattern sensitively integrating with the surrounding context.	Metal frame and curtain walls windows are proposed with muntin detailing that contribute to the scale of the fenestration.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
c)	Vinyl window systems are not permitted.	Vinyl windows are not permitted
d)	If the original windows have been removed, restoration shall be considered.	N/A – original windows cannot be preserved.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
e)	Windows that are blocked up in whole or in part shall be opened and properly reglazed.	N/A – windows are not blocked up
f)	Window openings that have been changed in size shall be returned to their original dimensions and appropriate window sash reconstructed.	N/A – the project is new construction.
g)	Where possible, original window and door hardware shall be conserved and reused.	N/A – the project is new construction.
12.	Colour	
	The choice of a colour scheme for a building shall be made on the basis of what is most appropriate for the individual building and also what colours may be compatible with adjacent buildings. Respect for nearby buildings will help to give a unified appearance within the Heritage Conservation Area	The project utilizes key highlights of colour as is prevalent among existing buildings in the area. A relatively neutral colour palette is utilized throughout the remainder of the building and the mass timber structure – a nod to the use of wood in the area – is left unfinished to enhance the relationship with the wooden architecture of the area.
13.	Interior Features	
	While these guidelines do not apply to the interior of buildings, owners, or designates, are encouraged to restore or retain historic interiors in a manner that is complementary to exterior façades	During demolition, portions of the building that can be retained are intended to be re-installed on the interior of the building.
14.	Lighting	
a)	All site lighting installations shall be fully shielded (full cut-off).	The proposal uses fully shielded fixtures for site lighting.
b)	Light shall be shielded such that the lamp itself or the lamp image is not directly visible outside the property perimeter.	The proposal will comply with this requirement.
c)	Security and other lighting shall be placed so as not to shine directly into residential properties or to reduce the separation effectiveness of any landscaped buffer.	The proposal will comply with this requirement.
d)	Exterior building lighting shall generally be concealed in soffits or other similar architectural features, where feasible.	The proposal will comply with this requirement.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
e)	Lamp poles and luminaries used for site area lighting shall be complementary to the form and character of adjacent sites and Village Streetlighting standards.	N/A - Lamp poles and luminaires are not contemplated.
f)	There shall be sufficient on-site illumination for pedestrian/vehicle safety.	The proposal will comply with this requirement at the rear enclosed parking lot and the public courtyard.
g)	Adhering to the principles of Crime Prevention through Environmental Design Principles (CPTED), lighting for pedestrian pathways, building entrances and parking areas shall be designed at a human scale (i.e., low level bollards) and address pedestrian safety, in a historically sensitive manner.	All lighting in the courtyard and at the enclosed parking lot will be specified to be sensitive to the human scale.
15.	Signage (Subject to compliance with Village’s Zoning Bylaw sign regulations)	
a)	The following sign types are recommended for use in the HCA designated area:	
i)	Projecting hanging signs: These signs project out from a building at a 90°angle and generally hang over the sidewalk. Projecting hanging signs shall hang no less than 3.0metres above the sidewalk surface and shall be constructed of wood or metal and be painted, enameled or carved by sandblasting. (Permission from the Village is required to construct a sign encroaching on public property).	Signs will be provided during the tenant fitout phase and can be hung from the principal building with agreement from the Village on an encroachment over the sidewalk.
ii)	Fascia signs: Fascia signs shall be installed over the main entry of a building. They shall be constructed of wood and painted. Installation of backlit plastic fascia signs is prohibited.	The primary sign for the residential portion of the building is a fascia sign adjacent to the lobby and visible from the primary intersection. The sign will be constructed of wood, painted, and gently lit under the covered entry.
iii)	Painted window signs: These signs are painted on the inside of windows. They shall be simple in design and painted in gold leaf or with drop shadows.	N/A – these signs may occur during the fitup.
iv)	Painted wall signs: Historically, businesses in Cumberland painted signs on the exterior	N/A – these signs may occur during the fitup.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
	walls of their buildings. Painted wall signs can be effective although caution shall be taken to avoid large garish brightly coloured wall signs that would be out of place in Cumberland.	
v)	Canopy or awning signs: The valances of canopies and awnings are often used for signage. Generally, the sign message shall be confined to the valance area of the awning or canopy although logos may be placed on the angled surface of the awning or canopy.	N/A – these signs may occur during the fitup.
b)	No signs shall be equipped with flashing, oscillating or moving lights or beacons.	The proposal has none of these features.
c)	Sign illumination for new signs shall be exterior, from the front and downcast. Internally illuminated signs may include halo lighting or the direct back lighting, only for individual letters and logos	Halo lighting for individual letters and logos is contemplated for the main sign to the residential development.
d)	No signs shall be permitted on a roof.	The proposal has no roof based signs.
e)	The following materials are recommended for signage:	
i)	Painted or sandblasted wood;	The specifications of the signs are being confirmed but will comply with the recommendations.
ii)	Painted or enameled metal;	The specifications of the signs are being confirmed but will comply with the recommendations.
iii)	Neon (appropriately designed in a period style);	Neon signs are not contemplated for any of the future developments.
iv)	Painted signs on fabric canopies or awnings.	The specifications of the signs are being confirmed but will comply with the recommendations.
16.	Access, Parking and Amenity Areas	
a)	Ensure the safe, efficient, convenient, and functional movement of multiple modes of transportation with priority given to alternate modes of transportation, including public transit, bicycles, and pedestrians.	The site fronts Dunsmuir so all of the public transit routes travelling on that thoroughfare are available to the site. The site has substantial private bicycle parking and provisions for public bicycle parking.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
b)	Promote the safety and mobility of through traffic by minimizing the number of access and egress points.	There is only one vehicular access to and from the site.
c)	Parking areas shall clearly identify pedestrian circulation areas, preferably with different paving and landscaping treatment.	Drive aisles are minimised through a multi-park system further enhancing safety. Pedestrian circulation is easily discernable.
d)	Parking areas shall be surfaced with a paving treatment (including: pervious paving, gravel with binders to minimize dust, cellular paving and concrete unit pavers).	Parking areas are replaced with more sustainable enclosed parking lot systems (multi-park system).
e)	Owners are encouraged to use permeable surface treatments for roadways, parking areas and other surfaced areas within a development.	Roadways are almost entirely eliminated on the site due to the parking system selected.
f)	Building entrances shall be prominent and clearly visible from access driveways, Dunsmuir Avenue or visitor/customer parking areas.	Building entrances are prominent and clearly visible from Dunsmuir Avenue and 2 nd Street. A single point of access is provided from the enclosed parking lot to the pedestrian courtyard which serves access to the residential and commercial parts of the development.

Heritage Alteration Permit Requirements		Development Proposal
11.2.2	HCA#1 Heritage Village Commercial Core	
	g) All developments must provide adequate garbage and recycling facilities located at the rear of buildings.	Garbage and recycling facilities are located at the rear of the building.
17.	Stormwater	
	a) A stormwater management plan shall be required as prepared by a registered professional engineer that has as its goal the maintenance of post-development flows equivalent to those of pre-development flow patterns and volumes over the entire wet weather season.	A stormwater management plan is provided.
	b) Stormwater management shall follow source control (on-site) principles and practices and minimize the use of conventional pipe and pond techniques, and avoid direct discharges to streams and other water-bodies.	A stormwater management plan follows these principles and practices.
	c) Stormwater management shall take advantage of on-site opportunities to recycle water to absorbent soils.	
18.	Soil Erosion and Sediment Control	
	A Sediment and Erosion Control Plan shall be prepared by a registered profession for the construction and operational phases of the development.	A sediment and erosion control plan is provided.



Corporation of the
Village of Cumberland

**DRAFT HERITAGE
ALTERATION
PERMIT**

2021-02-HAP

TO: Postmark Group

AGENT: Roddy Handa

OF: 8337 – 165 Street NW, Edmonton, Alberta, T5R 2R5

This Heritage Alteration Permit 2021-02-HAP is issued subject to compliance with all of the bylaws of the Village of Cumberland applicable thereto, except as supplemented by this Heritage Alteration Permit, for the purposes of constructing a four-storey mixed use building with commercial units on the ground floor and residential units above.

1. This Heritage Alteration Permit applies to and only to those lands within the Village of Cumberland described below, and any and all buildings, structures and other development thereon:

Legal Description: Lot 1, Block 6, District Lot 21, Nelson District, Plan 522

PID: 002-422-239

Civic Address: 2714 Dunsmuir Avenue, Cumberland, BC

and

Legal Description: The West ½ of Lot 2, Block 6, District Lot 21, Nelson District Plan 522

PID: 002-422-255

Civic Address: 2714 Dunsmuir Avenue, Cumberland, BC

2. The property be developed substantially in accordance with the following terms and conditions and provisions of this Permit:
 - a. The siting and design of the buildings will be as per the site plan and drawings attached to this Permit as Schedule A - Site Plan and Renderings.
 - b. The colours and materials of the buildings will be as per Schedule B – Colours and Materials.
 - c. Landscaping will be as per Schedule C – Landscape Plan.
 - d. Stormwater management on the site shall maintain post-development flows equivalent to those of pre-development flow patterns and volumes over the entire wet weather season.

Required before Building Permit is approved:

- e. Details on how the buildings will be made solar-ready.
- f. Lighting detail showing all lighting to be fully-shielded (full cut-off) and providing sufficient illumination for pedestrian and vehicle safety.

- g. Details of any signage (if available at the time) including dimensions and any projection.
- h. Registration of a 1.5metre wide Statutory-Right-of-Way (SRW) along the rear lane for the purposes of enabling the Village, in the future, to construct and maintain a lane of sufficient width to provide improved rear access, travel, and snow removal.
- i. Provision of two electric bicycle/scooter plug-ins associated with a bicycle/scooter parking space.
- j. Payment of cash-in-lieu of parking provisions as per the Zoning Bylaw requirements.
- k. Payment of applicable Development Cost Charges.
- l. Final Stormwater Management Plan

3. Security

To be determined upon submission of landscape plan.

4. Expiry

Subject to the terms of the Permit, if the Applicant of this Heritage Alteration Permit does not substantially start any construction with respect to which the Permit was issued within 2 years after the date it is issued, the Heritage Alteration Permit lapses.

5. Timing and Sequencing of Development

None.

6. List of Reports or Plans attached as Schedules

- a) Schedule A – Site Plan and Renderings
- b) Schedule B – Colours and Materials
- c) Schedule C – Landscape Plan

7. Contaminated Sites Regulation (*choose one of the following as applicable*)

This Permit is issued pursuant to the requirements of the *Environmental Management Act*, whereby the Applicant has completed a “Site Declaration” for the subject property.

OR

This Permit is issued pursuant to the requirements of the *Environmental Management Act*, whereby the Regional Manager of the applicable Ministry has received a “Site Profile” for the subject property and has issued a clearance to proceed to the Village on (*insert date*).

8. This Permit is not a Building Permit.

CERTIFIED as the **HERITAGE ALTERATION PERMIT** approved for issuance by resolution of the Council of the Corporation of the Village of Cumberland on _____ 2021.

Corporate Officer

Schedule A: Site Plan and Renderings

Notes:

- Check scale drawings.
- It is the responsibility of the Contractor to check and verify all dimensions on the site and report all errors and/or omissions to the Architect or Engineer as soon as possible.
- It is the responsibility of the appropriate Contractor to comply with all Codes and Regulations applicable to the performance of their work.
- All drawings are the property of ACI Architects Inc. They shall not be reproduced, stored in a retrieval system, or transmitted in any form or by any means, electronic, mechanical, photocopying, recording, or by any information storage and retrieval system, without the express written consent of the Architect or Engineer.

Project: THE EDDIE
Client:

Scale	As Indicated	Designed By	EL
Project No.	1111	Drawn By	BJUD
Date	20210521	Checked By	ZHI

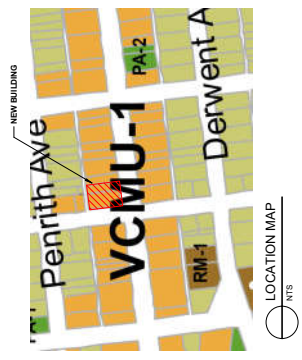
SITE PLAN - OVERALL

Drawing No. **A1.1**



THE EDDIE

1 OVERALL SITE PLAN
 SCALE: 1:200
 NORTH



Zoning Bylaw Review
 Municipality: 145 Derwent Avenue
 Legal Address: L.S. 1, Block 6, D.L. 21, Plan W9222
 Site Area = 1096 m²

MAIN FLOOR GROSS FLOOR AREA = 489 m²
 SECOND FLOOR GROSS FLOOR AREA = 489 m²
 THIRD FLOOR GROSS FLOOR AREA = 483 m²
 FOURTH FLOOR GROSS FLOOR AREA = 443 m²
 TOTAL GROSS FLOOR AREA = 1819 m²

Zone - VCMU-1 Village Commercial Mixed Use

6.2 Required Number of Vehicular Parking Stalls
 Minimum Vehicle Stalls = 15 stalls
 Min. Vehicle Stall Width = 1.55 m
 Min. Vehicle Stall Depth = 1.5 m

PROPOSED NUMBER OF PARKING STALLS = 14 STALLS

6.4 Required Number of Bicycle Parking Stalls
 Sources Controlled - 1.50 (with 1 Unit = 2 Stalls)
 Uncontrolled - 0.50 (with 1 Unit = 1 Stall)
 PROPOSED NUMBER OF SECURED PARKING STALLS = 48 STALLS
 PROPOSED NUMBER OF UNSECURED PARKING STALLS = 2 STALLS

9.4 VCMU-1 Village Commercial Mixed-Use Zone

1 - Principal Uses - Mixed use building
 Density - floor area ratio 2.0
 F.A.R. = 1.9 FLOOR AREA (M²) = 1618 m² / 1105 m²

6. Lot Coverage - Maximum 75%
 PROPOSED LOT COVERAGE:
 68% m² = 69%

7. Principal Buildings and Structures
 Minimum Front Setback - 0m
 Minimum Side Setback - 4.5m
 Minimum Rear Setback - 4.5m
 Minimum Side Setback - 0m
 Minimum Rear Setback - 0m
 Maximum Height - 15.0m

d) Despite the above, the maximum height may be exceeded for the following, provided the portion of, or projection from, buildings must not exceed vi) solar and heat towers

PROPOSED REAR SETBACK = 2.10m FOR 60% OF THE BUILDING FACADE
PROPOSED REAR SETBACK = 4.5m (OPTION A - AS SHOWN)
PROPOSED REAR SETBACK = 4.5m (OPTION B - AS SHOWN)
PROPOSED BUILDING HEIGHT = 15.0m
HEIGHT TO FINISH FLOOR OF FIRST FLOOR = 18.4m



1 OVERALL SITE PLAN
 SCALE: 1:200
 NORTH

Note:
 - Check all trade drawings.
 - It is the responsibility of the Contractor to check and verify all dimensions and to report all errors and/or omissions to the Architect or Engineer as soon as they are discovered.
 - It is the responsibility of the appropriate Contractor to comply with all Codes and Regulations applicable to the performance of their work.
 - All drawings are the property of ACI Architects Inc. and shall remain the property of the Architect or Engineer. This drawing is the Copyright of ACI Architects Inc. and shall not be reproduced or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording, or by any information storage and retrieval system, without the express written consent of the Architect or Engineer.

1 Issued for Development Permit 2021-03-19



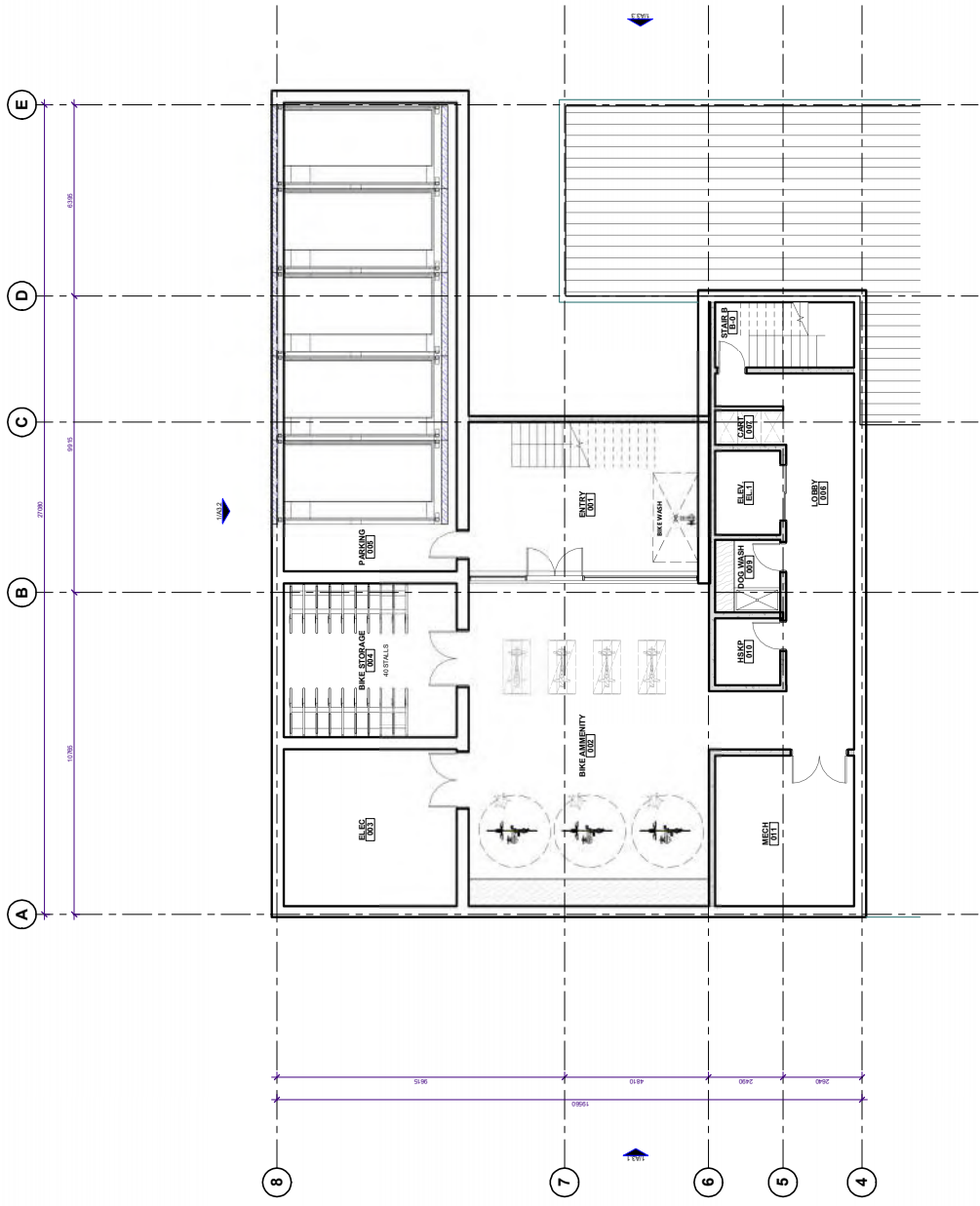
Client
THE EDDIE

Scale	1:75	Designed By	EL
Project No.	1111	Drawn By	BLJD
Date	2021-05-21	Checked By	ZM

**BASEMENT -
OVERALL PLAN**

Drawing No.

A2.0



1 BASEMENT FLOOR PLAN
 SCALE 1:75

Notes:

- Detail scale drawings.
- It is the responsibility of the Contractor to check and verify all dimensions and to report all errors and/or omissions to the Architect or Engineer as soon as they are discovered.
- It is the responsibility of the appropriate Contractor to comply with all Codes and Regulations applicable to the performance of their work.
- All work shall be done in accordance with the applicable Codes and Regulations of the City of Edmonton. This shall include the City of Edmonton's Building Bylaw and the City of Edmonton's Land Use Bylaw. The contractor shall be responsible for obtaining all necessary permits and approvals from the City of Edmonton and the relevant authorities.
- The contractor shall be responsible for the protection of all existing utilities and structures on the site.
- The contractor shall be responsible for the removal and disposal of all debris and waste materials from the site.
- The contractor shall be responsible for the maintenance of the site during the construction period.
- The contractor shall be responsible for the safety of all workers and the public during the construction period.
- The contractor shall be responsible for the completion of the project within the agreed-upon schedule.
- The contractor shall be responsible for the quality of the workmanship and materials used in the project.
- The contractor shall be responsible for the payment of all taxes and fees associated with the project.
- The contractor shall be responsible for the insurance of the project.
- The contractor shall be responsible for the bonding of the project.
- The contractor shall be responsible for the compliance with all applicable laws and regulations.
- The contractor shall be responsible for the coordination with all other trades and professionals involved in the project.
- The contractor shall be responsible for the communication with the Architect or Engineer throughout the project.
- The contractor shall be responsible for the submission of all required documentation to the relevant authorities.
- The contractor shall be responsible for the completion of all required inspections and approvals.
- The contractor shall be responsible for the final cleanup and site restoration.
- The contractor shall be responsible for the final handover of the project to the owner.

1 Issued for Development Permit 2021-03-19



Client
POSTMARK.

Project
THE EDDIE

Scale	1:75	Designed By	EL
Project No.	1111	Drawn By	BJLD
Date	2021-05-21	Checked By	ZHI

MAIN FLOOR - OVERALL PLAN

Drawing No.
A2.1



1 MAIN FLOOR PLAN
 SCALE 1:75

Notes:

- Check all trade drawings.
- It is the responsibility of the Contractor to check and verify all dimensions and to report all errors and/or omissions to the Architector as soon as possible.
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1 Issued for Development Permit 2021-03-19



Client: **THE EDDIE**

Scale	1:75	Designed By	BL
Project No.	1111	Drawn By	BLJD
Date	2021-05-21	Checked By	ZOI

SECOND FLOOR - OVERALL PLAN

Drawing No. **A2.2**



1 SECOND FLOOR PLAN
 SCALE 1:75

Note:
 - Check all trade drawings.
 - It is the responsibility of the Contractor to check and verify all dimensions on the site and report all errors and/or variances to the Architector as soon as possible.
 - It is the responsibility of the appropriate Contractor to comply with all Codes and Regulations applicable to the performance of their work.
 - All work shall be done in accordance with the applicable Codes and Regulations of the Province of Alberta and the City of Edmonton. This drawing is the property of ACI Architects Inc. and shall remain the property of ACI Architects Inc. All rights reserved. No part of this drawing may be reproduced or transmitted in any form or by any means without the express written consent of the Architect or Engineer.

1 Issued for Development Permit 2021-03-19



Project: **THE EDDIE**

Scale	1:75	Designed By	BL
Project No.	1111	Drawn By	BL/JD
Date	2021-05-21	Checked By	JDH

THIRD FLOOR - OVERALL PLAN

Drawing No.

A2.3



03 - THIRD FLOOR
 SCALE 1" = 8'
 1/8" = 1'-0"



Notes:

- Check all trade drawings.
- It is the responsibility of the Contractor to check and verify all dimensions and to report all errors and/or omissions to the Architect or Engineer as soon as they are discovered.
- It is the responsibility of the appropriate Contractor to comply with all Codes and Regulations applicable to the performance of their work.
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1 Issued for Development Permit 2021-03-19



Client: **THE EDDIE**

Scale	1:75	Designed By	EL
Project No.	1111	Drawn By	BLJD
Date	2021-05-21	Checked By	JSH

FOURTH FLOOR - OVERALL PLAN

Drawing No. **A2.4**



FOURTH FLOOR PLAN
 SCALE 1:75



Notes:

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1 Issued for Development Permit 2021-03-19

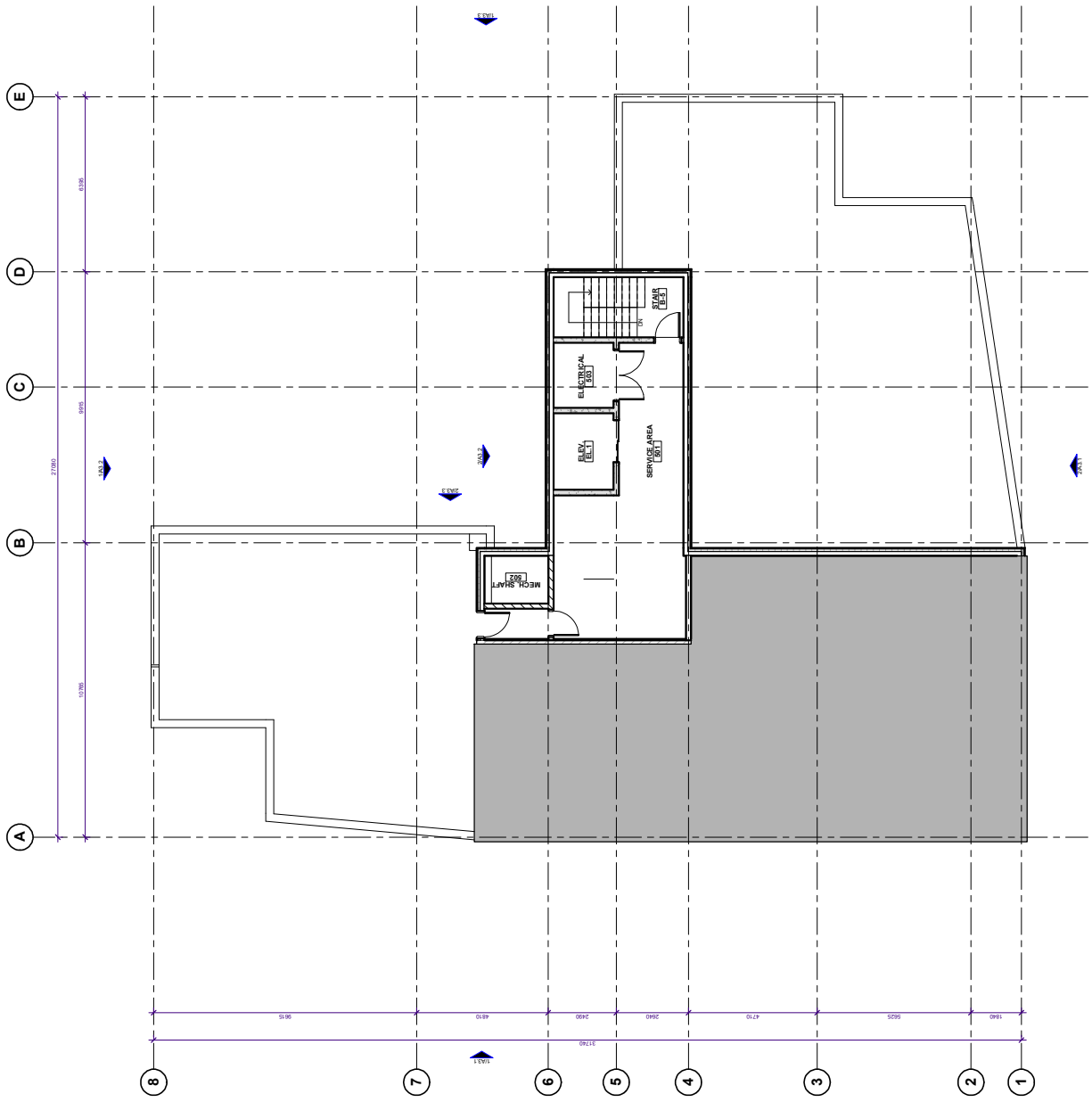


Client
 Project: **THE EDDIE**

Scale	1:75	Designed By	EL
Project No.	1111	Drawn By	BLJD
Date	2021-05-21	Checked By	ZHI

**PENTHOUSE -
 OVERALL PLAN**

Drawing No. **A2.5**



1 PENTHOUSE FLOOR PLAN
 1/8" = 1'-0" SCALE 1:75
 NORTH

Notes:
 • Check all trade drawings.
 • It is the responsibility of the Contractor to check and verify all information for accuracy and to report all errors and/or omissions to the Architector immediately.
 • It is the responsibility of the appropriate Contractor to comply with all Codes and Regulations applicable to the performance of their work.
 • The Architector is not responsible for the design of any mechanical, electrical, plumbing, fire, or other systems. The design of such systems shall be the responsibility of the appropriate professional engineer or other qualified professional.
 • The Architector is not responsible for the design of any foundation or other structural elements. The design of such elements shall be the responsibility of the appropriate structural engineer or other qualified professional.
 • The Architector is not responsible for the design of any interior or exterior finishes. The design of such finishes shall be the responsibility of the appropriate finish contractor or other qualified professional.
 • The Architector is not responsible for the design of any landscape or site work. The design of such work shall be the responsibility of the appropriate landscape architect or other qualified professional.
 • The Architector is not responsible for the design of any other trades or disciplines. The design of such trades or disciplines shall be the responsibility of the appropriate professional or other qualified professional.
 • The Architector is not responsible for the design of any other trades or disciplines. The design of such trades or disciplines shall be the responsibility of the appropriate professional or other qualified professional.

1. Issued for Development Permit 2021-03-19



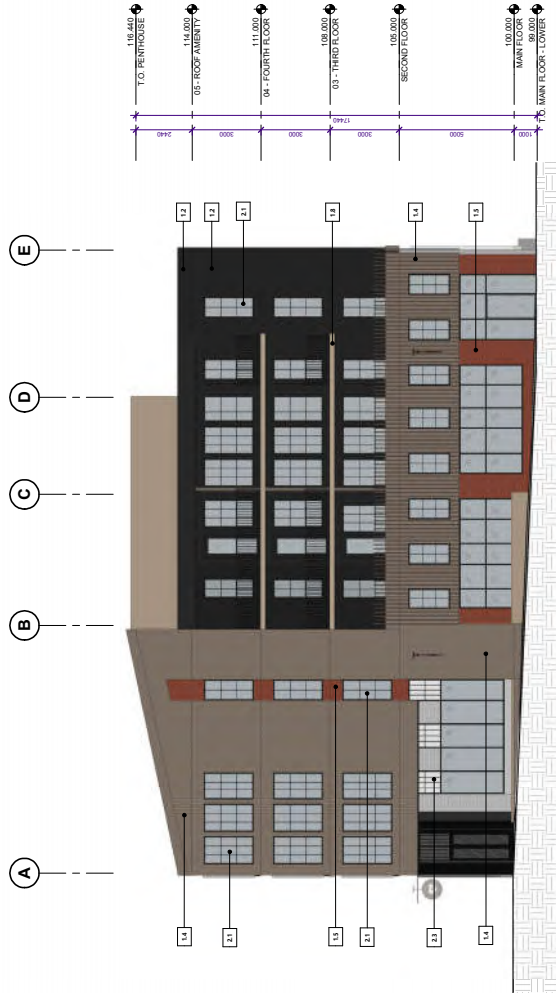
Client: **THE EDDIE**

Scale	1:100	Designed By	EL
Project No.	1111	Drawn By	EL
Date	2021-05-21	Checked By	JSH

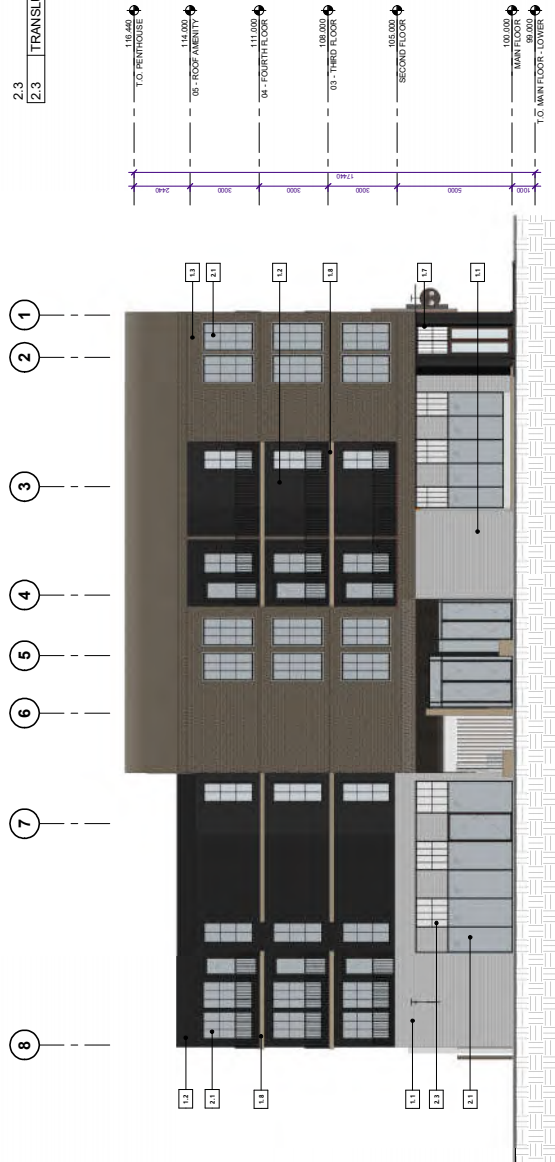
EXTERIOR ELEVATIONS

Drawing No. **A3.1**
 Drawing No.

MARK	EXTERIOR FINISH SCHEDULE DESCRIPTION	Material Area
	GROSS LAMINATED TIMBER STRUCTURE (EXPOSED)	335 m ² 335 m ²
1.1	TEXTURED CEMENTITIOUS PANEL	380 m ² 380 m ²
1.2	CHARRED WOOD PLANKS (SYNTHETIC NON-COMBUSTIBLE)	614 m ² 614 m ²
1.3	WOOD SHINGLE (SYNTHETIC NON-COMBUSTIBLE)	379 m ² 379 m ²
1.4	VERTICAL WOOD SIDING (SYNTHETIC NON-COMBUSTIBLE)	830 m ² 830 m ²
1.5	FIBRE CEMENT PANEL	31 m ² 31 m ²
1.6	FIBRE CEMENT PANEL	13 m ² 13 m ²
1.7	BLACK ANODIZED ALUMINUM FRAMES	654 m ² 654 m ²
1.8	GROSS LAMINATED TIMBER STRUCTURE (EXPOSED)	3948 m ² 3948 m ²
2.1	INSULATED GLASS - CLEAR	681 m ² 681 m ²
2.3	TRANSLUCENT KALWALL GLADDING	62 m ² 62 m ² 7929 m ²



2 SOUTH ELEVATION
 SCALE 1:100



1 WEST ELEVATION
 SCALE 1:100

Notes:

- Check all elevations.
- It is the responsibility of the Contractor to check and verify all dimensions and to report all errors and/or omissions to the Architector as soon as possible.
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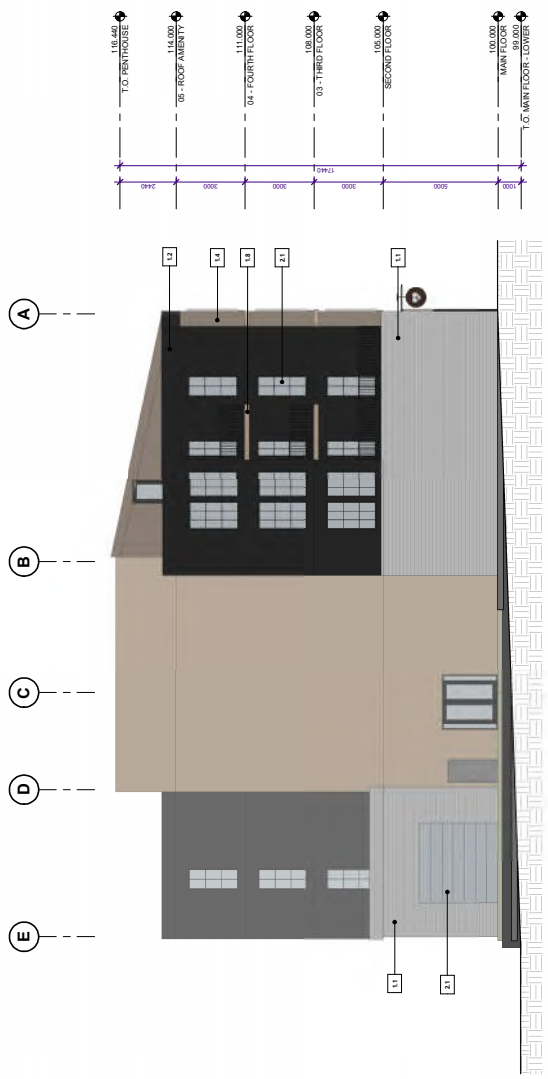
Client
 Project: **THE EDDIE**

Scale	1:100	Designed By	EL
Project No.	1111	Drawn By	EL
Date	2021-05-21	Checked By	ZOI

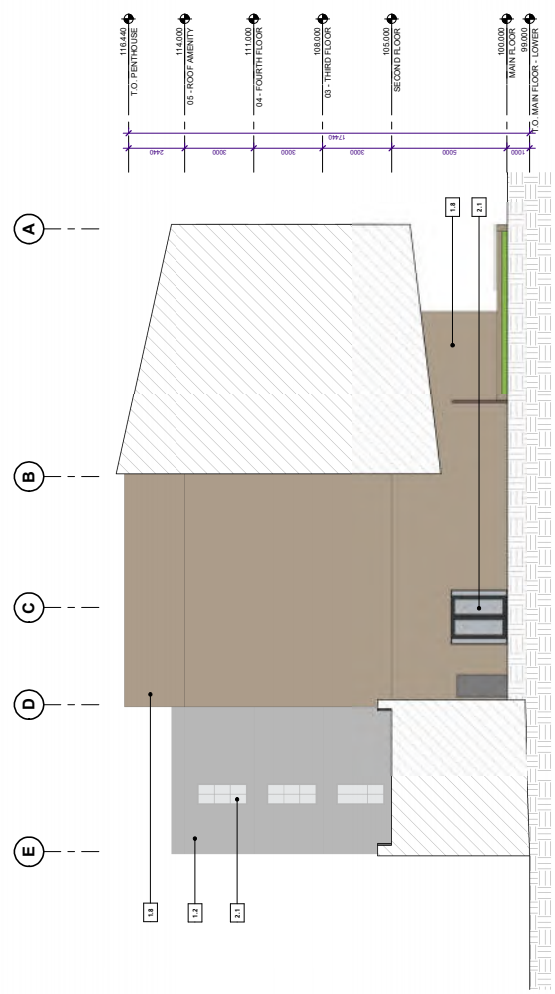
EXTERIOR ELEVATIONS

Drawing No. **A3.2**

EXTERIOR FINISH SCHEDULE		
MARK	DESCRIPTION	Material / Area
	CROSS LAMINATED TIMBER STRUCTURE (EXPOSED)	335 m ² 335 m ²
1.1	TEXTURED CEMENTITIOUS PANEL	380 m ²
1.2	CHARRED WOOD PLANKS (SYNTHETIC NON-COMBUSTIBLE)	614 m ² 614 m ²
1.3	WOOD SHINGLE (SYNTHETIC NON-COMBUSTIBLE)	379 m ² 379 m ²
1.4	VERTICAL WOOD SIDING (SYNTHETIC NON-COMBUSTIBLE)	830 m ²
1.5	FIBRE CEMENT PANEL	31 m ² 31 m ²
1.6	FIBRE CEMENT PANEL	13 m ² 13 m ²
1.7	BLACK ANODIZED ALUMINUM FRAMES	654 m ² 654 m ²
1.8	CROSS LAMINATED TIMBER STRUCTURE (EXPOSED)	3948 m ² 3948 m ²
2.1	INSULATED GLASS - CLEAR	681 m ² 681 m ²
2.3	TRANSLUCENT KALWALL CLADDING	62 m ² 62 m ² 7929 m ²



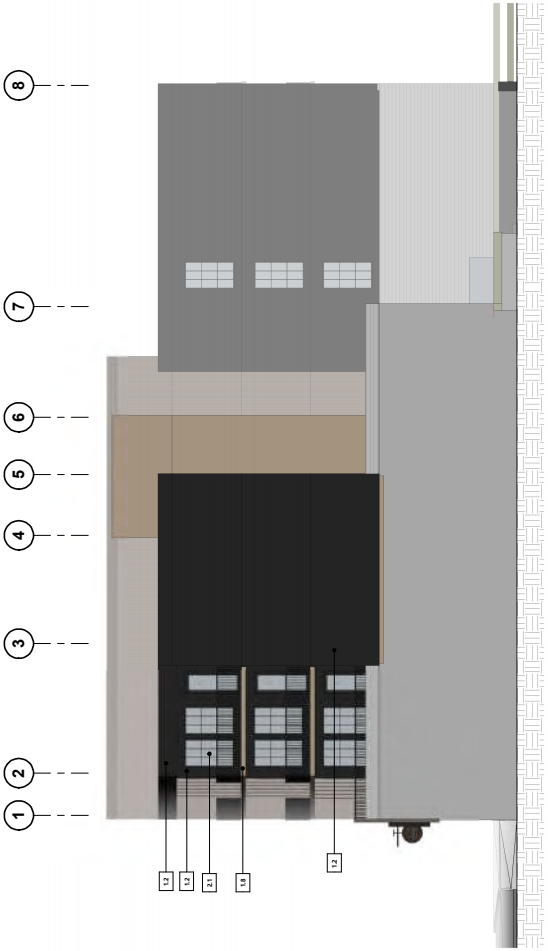
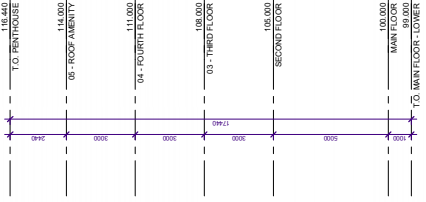
1. NORTH ELEVATION
 A3.0 / SCALE 1:100



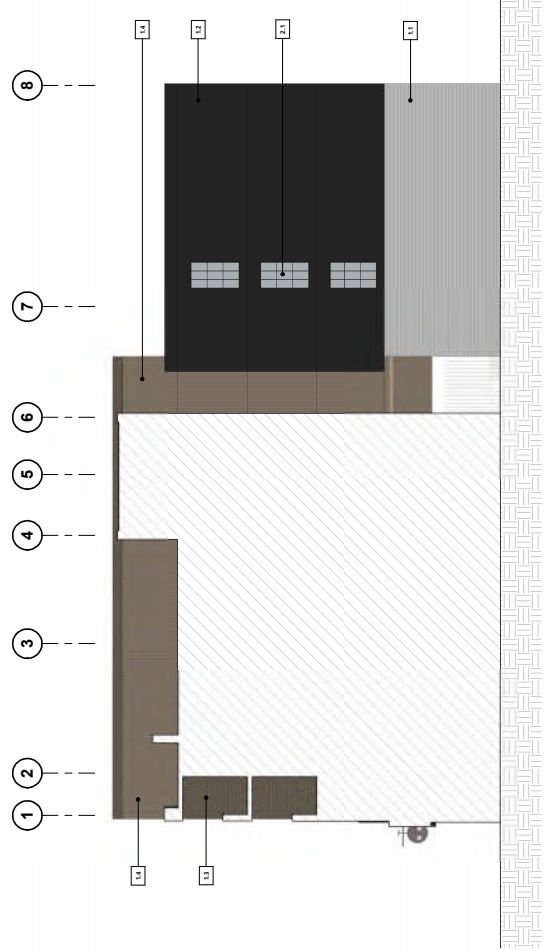
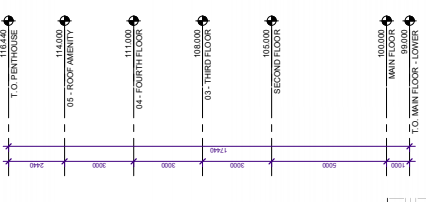
2. NORTH ELEVATION - COURTYARD
 A3.1 / SCALE 1:100

• We warrant that the work shall be performed in accordance with the contract documents.
 • We warrant that the work shall be performed in accordance with the contract documents.
 • We warrant that the work shall be performed in accordance with the contract documents.
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MARK	DESCRIPTION	Material Area
	CROSS LAMINATED TIMBER STRUCTURE (EXPOSED)	335 m ² 335 m ²
1.1	TEXTURED CEMENTITIOUS PANEL	380 m ²
1.2	CHARRED WOOD PLANKS (SYNTHETIC NON-COMBUSTIBLE)	614 m ²
1.3	WOOD SHINGLE (SYNTHETIC NON-COMBUSTIBLE)	379 m ² 379 m ²
1.4	VERTICAL WOOD SIDING (SYNTHETIC NON-COMBUSTIBLE)	830 m ²
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1.8	CROSS LAMINATED TIMBER STRUCTURE (EXPOSED)	3948 m ² 3948 m ²
2.1	INSULATED GLASS - CLEAR	681 m ² 681 m ²
2.3	TRANSLUCENT KALWALL CLADDING	62 m ² 62 m ² 7929 m ²



1 EAST ELEVATION
 SCALE 1:100



2 EAST ELEVATION - COURTYARD
 SCALE 1:100

1 Issued for Development Permit 2021-03-19



Project: THE EDDIE

Scale	1:100	Designed By	EL
Project No.	1111	Drawn By	EL
Date	2021-05-21	Checked By	DEL

Exterior Elevations

Drawing No. **A3.3**



PERSPECTIVE VIEW FROM DUNSMUIR & 2ND



PERSPECTIVE VIEW AT OUTDOOR PATIO SPACE ALONG DUNSMUIR



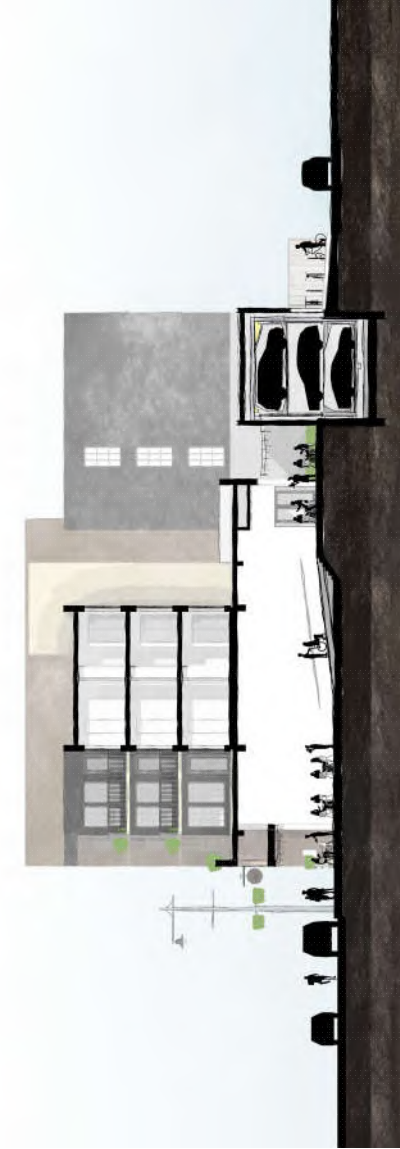
SOUTH ELEVATION STREETSCAPE



WEST ELEVATION STREETSCAPE



BUILDING SECTION (EAST/WEST) AT RESIDENTIAL ENTRANCE AND REAR COURTYARD AREA



BUILDING SECTION (NORTH-SOUTH) AT OUTDOOR SEATING SPACE ON DUNSMUIR, COURTYARD AREA

Schedule B: Colours and Materials



Schedule C: Landscape Plan

To be submitted and attached to final permit



2021-11 DV

TO: Postmark Group

AGENT: Roddy Handa

OF: 8337 – 165 Street NW, Edmonton, Alberta, T5R 2R5

This Development Variance Permit (2021-11-DV) is issued subject to compliance with all of the bylaws of the Village of Cumberland applicable thereto, except as specifically varied or supplemented by this Permit for the purposes of varying the rear setback of the building and reducing the parking requirement.

1. This Development Variance Permit applies to and only to those lands within the Village of Cumberland described below, and the proposed four storey building thereon:

Legal Description: Lot 1, Block 6, District Lot 21, Nelson District, Plan 522

PID: 002-422-239

Civic Address: 2714 Dunsmuir Avenue, Cumberland, BC

and

Legal Description: The West ½ of Lot 2, Block 6, District Lot 21, Nelson District Plan 522

PID: 002-422-255

Civic Address: 2714 Dunsmuir Avenue, Cumberland, BC

2. The land described herein shall be developed substantially in accordance with the following terms and conditions and provisions of this Permit. Zoning Bylaw No. 1027, 2016 is varied as follows:

Part 6 Parking requirements are reduced to _____ spaces.

Section 9.4 The minimum rear setback of 4.5m is varied to 1.5 m.

3. **Security**

None.

4. **Expiry**

Subject to the terms of the Permit, if the Applicant of this Development Variance Permit does not substantially start any construction with respect to which the Permit was issued within 2 years after the date it is issued, the Permit lapses.

5. **Timing and Sequencing of Development**

None.

6. List of Reports or Plans attached as Schedules

Schedule A: Building and Site Plan

7. Contaminated Sites Regulation (*choose one of the following as applicable*)

This Permit is issued pursuant to the requirements of the *Environmental Management Act*, whereby the Applicant has completed a "Site Disclosure Statement" for the subject property.

OR

This Permit is issued pursuant to the requirements of the *Environmental Management Act*, whereby the Regional Manager of the applicable Ministry has received a "Site Profile" for the subject property and has issued a clearance to proceed to the Village on (*insert date*).

8. This Permit is not a Building Permit.

CERTIFIED as the DEVELOPMENT VARIANCE PERMIT approved and issued by resolution of the Council of the Corporation of the Village of Cumberland on _____, 2021.

Corporate Officer

Schedule A: Site Plan

Site plan showing a 1.5 m setback to be attached

Heritage Alteration Permit and Development Variance for 2714 Dunsmuir Avenue

Comments from Public Information Meeting held Wednesday, August 4, 2021, from 4 pm to 6 pm	
Address	Comment
████ Penrith Avenue	<ol style="list-style-type: none"> 1. The loss of the Cumberland Hotel means we lose a venue for artistic performance. Please try for some large performance space 2. Parking and delivery truck loading: some increase for the commercial customers parking please – the neighbourhood adjacent is residential. 3. Great work on the viewscape – good massing placement, love the courtyard, nice “shout-out” to the 2-story line of the adjacent commercial buildings.
████ Penrith Avenue	<ol style="list-style-type: none"> 1. It is a lovely design for multiple dwellings structure. However it will change the “curb appeal” for our little village. 2. The key concern is PARKING! We are already facing a shortage of parking on Penrith. 3. Our street is very closed to downtown and parking on the weekends is a challenge for the residents on Penrith. 4. The serious concern is safety for the children on our street. 5. In sum: parking & traffic, safety should be our priority. Thank you for sharing your proposal
████ Second Street	<p>If there was any chance the commercial space could be combined into one room to allow for restaurant/music room similar to the original Cumberland Hotel, it would meet the cultural needs of the existing community. Music that has historically been performed in that room is part of the “heart and soul” of this community.</p>
████ First Street	<ol style="list-style-type: none"> 1. Design has no heritage merit 2. “Eddy” has not Cumberland context 3. No to variance 4. Revisit design as a gesture of goodwill to the community??
████ Derwent Avenue	<ol style="list-style-type: none"> 1. I’m concerned about the parking variance as I live one block away and believe overflow parking will affect my ability to park near my residence. 2. This building looks kind of boxy and monolithic for downtown Cumberland (Also, I wish the plans included a bar/lounge with karaoke, haha) 3. Could this plan have included some units of affordable housing?

**Comments from Public Information Meeting held
Wednesday, August 4, 2021, from 4 pm to 6 pm**

Address	Comment
<p>█████ Dunsmuir Avenue</p>	<ol style="list-style-type: none"> 1. I appreciate the extensive use of “shingles” on the façade (facing Second Street & Dunsmuir) 2. The East and South step-backs along Dunsmuir soften the building’s imposing stature. 3. Keeping the old entryway (angled like the old Hotel) is a nice touch. Is there any other ways to have a “shout-out” to the old Hotel? 4. Can the North façade on Second Street be stepped up similar to the East & South along Dunsmuir? 5. Please offer to Comox Valley residents first. *No short-term rentals allowed.
<p>No address listed</p>	<ol style="list-style-type: none"> 1. Should be named after something local – not vain glorious naming after oneself. 2. The Hotel should not be torn down to begin with but if it is then as much of the original materials should be used to help & be incorporated into the new building.
<p>█████ Maryport Avenue</p>	<ol style="list-style-type: none"> 1. The esthetic of the building does not match the Village’s character. It looks like a box & a high box at that. Even some gingerbread, or hardiplank shakes would help. – My apologies as I have it explained that hardiplank is being used. 2. I am glad to see that it is wheelchair accessible.
<p>█████ Maryport Avenue</p>	<p>I find the exterior presentation to be lacking character. The nod’s to Cumberland are token at best and the building doesn’t fit the heritage of the community.</p>
<p>█████ Penrith Avenue</p>	<p>I am very concerned about parking & increased traffic on Second Street and Penrith Avenue. I realize that parking is one spot for each unit (almost), but what about guests, 2 car families, employees ad patrons?</p>
<p>█████ Second Street</p>	<p>We live ██████████ from the Cumberland Hotel, and our business (Uppercut Barbershop) was already displaced by this project, so we’re quite critical at the outset. We are sad to be losing a heritage building and frankly not thrilled by the design that’s been presented. While there are nods to Cumberland’s history, it stands out like a modern sore thumb from the character that makes this village unique from other towns/cities. That aside, we are very concerned about the increase of traffic along Second Street, as there are numerous families with young children in the nieghbourhood. As well, we are already facing difficulties with parking, given the mixed residential/commercial nature of the block. Assuming that most of the people buying units will likely have more than 1 vehicle, I don’t see how providing only 14 parking spaces will even begin to address the influx of vehicles to this area. We would really like to hear some proactive solutions to this issue. Thank you.</p>

**Comments from Public Information Meeting held
Wednesday, August 4, 2021, from 4 pm to 6 pm**

Address	Comment
█████ First Street	<ol style="list-style-type: none"> 1. While I appreciate the heaviest building mass is on the corner of Second & Dunsmuir, I find the design to lack in consideration the village nature and quaint feel of our downtown core. Lack of timber in design, lack of decorative features and a “block-like” appearance is better suited to a larger city. It evokes images of downtown Vancouver. 2. I highly recommend against parking variance or in lieu-of-cash in light of the multitude of developments requesting this. It will only serve to push traffic and noise into residential streets at the expense of peace for existing members of the community.
█████ Bruce Street	<p>Note: All my references to the document O.C.P Bylaw #990, 204 HVCC p. 83 forward.</p> <p><u>Positives</u> – innovative in their approach to parking on site (according to architect) commercial spaces (bike & restaurant) have been spoken for by Cumberland.</p> <ul style="list-style-type: none"> -encourages densification and multiple use (11.2.1) -meets accessibility guidelines -materials respect & compliment adjacent structures (11.2.2(f)) <p><u>Negatives/Concerns</u> – in my opinion, does not conform or attempt to conform to the design objectives of the H.C.A. (11.2.2(e))</p> <ul style="list-style-type: none"> -the building form, nor its character reflect or respect the design of adjacent buildings (11.2.2.(b) p. 89 (11.1.2(e)) -builder did not adequately look into the unique & core for design cues and guidance. Basing the new structure on an industrial model (see pamphlet handed out) is not acceptable for the downtown core. (11.2.1(1)) p. 168 -no mention of how they will dispose of current structure. <p><u>Other Comments</u></p> <ul style="list-style-type: none"> - meeting was called on short notice, poorly advertised and during work hours. -pleased to see the volume of turnout despite the above mentioned. -I felt the developer was somewhat bullish and disrespectful in speaking with attendees and she did not appear to be listening when being spoken to. (eyes diverted, looking around the room). This gave the impression to some that it was a done deal. When questioned as to what would happen if council asked for changes, her response was there would be a lawsuit and lawsuits can be long and costly. (Not a good start in my opinion). <p>My main concern is the integrity of main street be continued as is, since this is what many who live here and those who visit see as “Cumberland”. If we lose this we lose a big part of who we are.</p> <p><u>Please consider this as you make your decision:</u></p> <p>The past 20 years has seen changes to the downtown commercial core – either through fires, demolition and remodeling. Despite this, the character of buildings and streetscape has remained mostly uniform on Dunsmuir Ave....This is a significant asset to the</p>

**Comments from Public Information Meeting held
Wednesday, August 4, 2021, from 4 pm to 6 pm**

Address	Comment
	village....Subsequently this area justifies stewardship, conservation and repurposing of the downtown commercial core buildings and streetscape. P.83, OCP Bylaw #990, 2014.

Heritage Alteration Permit and Development Variance for
2714 Dunsmuir Avenue

e-mail submissions

July 27, 2021 – Address not given

Hi,

I'm writing in response to the notice that I received for the HAP and DVP for 2714 Dunsmuir Avenue.

I have the following comments on the DVP:

- There is no need to reduce the parking requirements through the DVP process. Unless things have changed the applicants has the ability to pay Parking in Lieu (PIL) for the spaces they are not willing to put onsite. This is the proper process for the applicant to reduce parking requirements, and would help to contribute towards the long-term viability of parking in the Village Core.
- Any reduction of the rear yard setback needs to take into account the Village's requirement to service that alley. This alley space already has a small footprint and there are ongoing issues with maintenance. Further reducing the setback could negatively contribute to this problem.

In regard to the form and character of the HAP. It all looks fine to me based on the drawings - but there's honestly not enough information to make an informed decision/properly comment. What are the building materials? Will there be electric vehicle charging stations required? Is there a link to full sized renderings and relevant information on the HAP? That would make it easier to comment.

Thanks for the ability to provide input.

August 1, 2021 – Address not given

I am writing to express my concern regarding the proposed development of the Cumberland Hotel site.

If Cumberland is to market itself as a historic destination it must cease the 'uglification' of its main street.

The Rideout building is as ugly as this proposal and the character of the Dunsmuir Avenue is rapidly being destroyed.

This proposal is overly tall, they are asking for variances and, somehow, the developers are claiming to incorporate some of the features of the hotel but any of that escapes me in the 'artists' rendition.

This is an overly large square box with seemingly zero character, aesthetics or any other redeeming qualities.

Please send them back to the design table

Maybe it is time for Cumberland to invoke some design covenants into its by-law

August 1, 2021 - 2781 Penrith Avenue

I do not agree with having the number or parking spots required changed so they can maximize units in the building. Dunsmuir has a severe problem with a lack of street parking already and the other 2 buildings aren't even occupied yet. Where are you expecting all these people to park. I also feel that local businesses will suffer do to potential clients not being able to find parking.

Strongly opposes variances being granted

August 2, 2021 – Address not given

Hi

I live in Cumberland and would love to see the new development on main street be only 3 stories instead of a monster 4 story building

Parking will be and issue and as well as I think it will kinda ruin the look of our little village

I agree that we need to grow and people need spaces to live so I'm all for the new apartment but we don't want to end up Looking like a mini Vancouver

Thanks for your time

August 2, 2021 - [REDACTED] Mill Street

Hi there,

I'm writing today to voice my concern regarding the requested parking variance for the property at 2714 Dunsmuir. I don't see how 15 units can be added to this area without adding parking. It is likely that most of the residents of these units will own cars, given the relatively poor transit and cycling infrastructure between Cumberland and the rest of the Comox valley, and the need for accessing businesses and services throughout the valley.

With the likelihood of more medium to high density development (which I fully support) along Dunsmuir, it makes sense to me to establish a tradition of requiring parking on site for these developments early on, rather than trying to solve the problem in the future.

Preserving as many free public parking spaces as possible for visitors from the rest of the valley and beyond will also support our local businesses as they recover from the pandemic.

I write this email as someone who would love to see bike lanes, patios, and parklets replace the existing parking spaces. Nonetheless, I can see the folly in failing to preserve easy parking opportunities in an area that is so reliant on personal motor vehicles. Until such time as transit and cycling links to the Courtenay and Comox are reliable and safe, I kindly request that on site parking be a requirement for new developments.

August 2, 2021 – Address not given

To whom it may concern,

I am writing to express my deep concern for the style in which the Postmark Group is planning to construct their new building on the old Cumberland Hotel site.

Although I am aware that the existing building does not fall under the 'heritage site' protection of the Village, I still feel that we should be preserving the historical look of the downtown core with any building that replaces it.

The proposed Potmark Group building, that I have seen representation of, looks SO modern and is SO much larger than any other building downtown, I feel it will really change the look of the whole area.

I am all for diversification and densification of the downtown area, but I think that the Postmark Group could design something with more of a historic feel. I would also like to see them build in some levels, so that it doesn't just look like a big box. I am aware this would mean fewer suites and less money for the Group, but I believe allowing spaces for trees and gardens to be included in their design will ultimately keep the whole block (and their residents) from overheating in our ever changing climate.

I really hope the Village Council and planners will reconsider allowing this style of building in the downtown core and on one of the most visible corners.

August 3, 2021 – Address not listed

Hi,

I am emailing in response to the July 23rd letter we received regarding Heritage Alteration Permit and Development Variance for 2714 Dunsmuir Ave. We received the letter because we are the owners of the 'Tarbell's' building at 2705 Dunsmuir Ave.

I have the following comments in regards to the heritage alteration permit; the renderings provided show a modern building aesthetic that is not in keeping with the current heritage building on that site and other heritage buildings in the downtown core and looks out of place as a result. I am opposed to the modern design as presented.

I have the following comments in regards to the development variance request; any reduction in parking requirements should not be permitted. Parking is already extremely difficult in the downtown area. Adding 15 dwellings and 3 commercial spaces without the appropriate amount of parking will only serve to exacerbate the problem. I am opposed to the request to reduce parking requirements from 30 to 14 spaces.

I am also opposed to the requested reduction in rear setback from 4.5 to 1.5metre.

In summary, I am opposed to the application as presented. I would however be supportive of a redevelopment plan that meets the aesthetic of the downtown core and can meet the current parking and setback requirements as outlined in our bylaws.

August 5, 2021 - [REDACTED] Penrith Avenue

Hello,

I am writing in response to the proposed heritage alteration permit and development variance for 2714 Dunsmuir Avenue. While we are in favour of "building up" as opposed to

"building out" we do not support the proposed variance for the rear setback or the reduced parking requirement.

I would prefer to see the setback area used to grow a few trees (as shown in the rendering) for the benefits they provide (shade/temperature regulation, aesthetics etc).

With respect to the parking areas, it seems reasonable to assume that there will be at least one vehicle associated with each dwelling unit but likely more. The proposal does not provide enough parking spaces for the dwellings, let alone the commercial spaces. The likely eventuality is that this will lead to congested parking and more traffic in the surrounding residential streets.

Lastly, the scale and design of the building shown in the renderings does not appear to be in keeping with the character of the surrounding buildings. It's large, generic and, in my opinion, rather ugly.

In summary, this proposal appears as though it will have a negative impact in terms of traffic, parking, aesthetics and the overall livability of our neighbourhood. I would prefer they reduce the number of units, adhere to the setbacks and parking requirements, produce a more visually compelling design and plant the trees shown in the renderings.

August 5, 2021 – Address not given

An absolute affront to village residents that no rational person can honestly say is in line with the village character, and quite frankly an offensive lack of engagement for this entire process from the developer.

Even ignoring the blatant disregard of any design elements that would potentially make this work visually, this should absolutely require onsite parking with no exemptions, period. Local residents and tourism/recreational users should not be left to deal with the lack of parking simply because the developer would take a hit on their returns for building this.

The building is clearly overheight based on the surroundings, and based on comparative work they are engaged on on Sooke, would appear to be hoping to push this minimalist concrete and metal trim eyesore through unencumbered, in a small village with what seems to be increasingly lacking oversight on projects like this, countering what this very village bills their identity as being.

Letting this project move forward as is would nothing less than disgraceful on the part of the council.

August 5, 2021 – Address not given

I attended The Eddie Open House last night. I think it goes without saying that the hosts, Postmark and Holo-Blok, performed their roles incredibly well, justifying why The Eddie design "currently conforms with all heritage zoning guidelines". It is my hope that in deciding

the fate of this iconic building and irreplaceable location, more than just ticking guideline boxes will be considered.

I live in Coal Valley Estates. This is also another area where the Developer - according to the Mayor - followed all the regulations. What has resulted is growing suburban sprawl with narrow streets, too many cars due to suites, ever larger houses, no green space, and nary an indigenous tree or plant in sight. Even the Mayor replied to me that upon viewing the homes looking down on top of one another that "...it does look overwhelming, unfortunately they followed all the regulations and are legal." I dread that same perspective when thinking of the redevelopment of the Cumberland Hotel site. And I also understand there are legalities at play with respect to the Community and Postmark. That said, here is what I am struggling with...

I have done my best to educate myself on the ins and outs of this project and all the assorted bylaws, etc. It's not easy when you're working full time and have other life responsibilities and things like last night's Open House are not advertised. However, among everything I've looked at, what stands out for me is the **Part D, Section 11.0 – HCA1 – Historic Village Commercial Core (HVCC) , Official Community Plan, Bylaw #990** (<https://cumberland.ca/wp-content/uploads/2012/05/Part-D-Implementation.pdf>), specifically:

- 11.1.1. Justification; and,
- 11.1.2 Objectives.

In chatting with the architect from Holo-Blok, I remained absolutely unconvinced that their design – said to incorporate influence from past Cumberland industrial buildings, the old Cumberland Hotel, and even the Riding Fool Hostel – comes anywhere near meeting the spirit or intention of this document. Beyond the incompatibility with the legacy of the hotel, the design simply does not belong in Cumberland. It is an urban condo that belongs in a cityscape, not a Historic Village. To that end:

- I urge Council and the Planning Department to think hard about the long-term impact on the visual fabric and culture of the Village;
- I urge Council and the Planning Department to look to other smaller towns in BC that have maintained their character while shifting their economic model (i.e. Rossland, Ladysmith, Fort Langley);
- I urge Council to view the design through the intention and spirit of OCP, Part D, 11.0, HCA1; and,
- I urge Council to also reflect on the impact on the [2018-2023 Economic Development Strategy](#).

I am not anti-development but I am very development cautious because I used to work in that world and have experienced it from the inside. The reason I left the industry was I witnessed too many developers making enormous profits by significantly altering the landscape of places where they did not live, work, or play. The communities would be left with the legacy of what was built long after the developers vacated the scene. Please ensure

that the legacy of Postmark's time in in our village is the right one for the future – for the culture – of Cumberland. This will be looked upon as a Defining Moment.

One last request. Can the name be changed? While I appreciate the intention of one of Postmark's partners wanting to honour his late father, perhaps The Eddie name would be better suited in his hometown of Edmonton rather than Cumberland where it has literally nothing to do with our heritage.

Thank you for your time.

August 5, 2021 - [REDACTED] Maryport Avenue

Hi,

Thank you for taking my comments into consideration when reviewing the parking variance request from Postmark regarding the Eddie building.

I believe that the parking variance should NOT be approved. Developers are responsible for providing adequate parking. Period. There will be residents of this building, visitors to the residences, staff of the commercial spaces, and visitors to the commercial spaces who will all need somewhere to park.

The financial penalty of an approved parking variance is not adequate to deter the practice. So it is a variance request that must be denied. Please do not approve this and use the financial penalty to pad the village coffers, as it is a short term gain which will have long term repercussions to the appearance and ease of use of visiting the main street. Dunsmuir parking will start to overflow onto nearby residential streets where many homeowners already use the street or boulevard for their own parking.

I live nearby and therefore walk to businesses on Dunsmuir, so this issue has no direct parking impact on me, but I still feel very strongly about the ongoing availability of street parking at storefronts and empty spaces along the road during the evenings and for winter snowplowing. If we end up with vehicles permanently parked there, they will clog up the main street and eventually will require motivators to increase parking turnover such as hourly limits (therefore increasing the need for bylaw enforcement and administration) or pay parking (I hope we can manage to avoid doing this in the future- it breaks my heart that we will soon be watching the clock on our Robbins parking meter in order to visit a Tofino beach).

Thank you for your consideration of my comments,

August 6, 2021 – Address not given

Hello,

I just wanted to write a quick note about my thoughts on the proposed building to replace the existing Cumberland Hotel.

I own two properties in Cumberland and have lived here since 2012.

This building maintains absolutely none of the heritage character that the existing building has.

This type of redevelopment can not be allowed. Just imagine if every building on the main street of Cumberland was turned into this, that must be held top of mind by the planning and heritage committees. Precedent must be set so that NO building on our main street can be made with a flat roof as high as is allowed. As a minimum, there must be height variations at the roof line, and changes in floor footprints for each level. Otherwise, it's just a cube.

Each approved building must be such that it looks similar to the other buildings on main street, so that if every building on main street was replaced with a similar style, it would still look like our main street. This building very obviously does not meet this simple criteria.

Also, I think that no further parking allowances can be made. Parking must be included in all plans from now on, there is no amount of money that will overcome parking issues once they are out of control.

August 6, 2021 – Address not given

Dear Planning Committee,

I am writing to you to voice my concern and opposition of the variances proposed by the Postmark Group for the Eddie Building.

I believe that the proposed design is not in the Village's interests and if approved will be a turning point in preserving the charm and heritage of our Village that we all love so much.

My two biggest concerns at this point are the design (height and size) of the building and the request for a parking variance.

1 Parking- we all know how difficult it can be already to find parking in the village, now with three new developments coming here without adequate parking I know it is going to have an impact on current residents (especially living in the areas closest to these buildings), businesses will have less space for their employees and customers to park and therefore customers may stop coming, and visitors will not be able to find places to park.

I know there is a rental housing crisis but these condos are being marketed as luxury and will likely have a strata that may allow a few or may not allow rentals and will not likely be affordable for many renters if they are even able to rent. The type of clientele that be able to afford these units will most likely have 1-2 vehicles. This will most definitely impact available parking if there are not 2 spaces created per unit. I assume these residents will also have guests that will also require visitor parking. Additionally the commercial spaces they are creating will need staff parking and customer parking. I don't understand where an additional 40-50 cars could park without taking up nearby resident parking.

We own the Cumberland Guesthouse (formally Stansbury's Guesthouse) and feel that this will also directly impact the parking available for our guests as the need for parking will go up the street to our property and I assume take up spaces for the Riding Fool as well. This will

create a headache for accommodation customers/owners if there is no longer parking available.

Hypothetically, if the parking variance was turned down, would this force the developers to change their design and possibly lose a level of units if there isn't enough space for their own parking? Or would they just build higher?

Is there anything the residents of Cumberland can do to support the village to say no to this variance? I know that the majority of residents are against this parking variance, maybe as a community we can do some fundraising to help the village still earn the money that would have been paid for them not to have parking spaces? I feel that there are enough of us that are strongly opposed to this current proposal that the community would rally to do something to help if given the opportunity. Please let us know how we can help.

Moving forward I would strongly advise some more rules about what is allowed to be built on our main street. It may be a good idea to look around to what other communities have put in place to protect their Historic and Eclectic vibe. Canmore may be a good example to research.

#2 Building design- I have researched this developer, they make beautiful buildings but this proposed design does not look like it will fit into our current Dunsmuir aesthetic. The height and size of this building is so massive that it will ruin the charm that everyone loves about Cumberland. I don't have a problem with them creating new commercial spaces but I guess I wish that the vacant commercial spaces would be filled before thinking about creating new ones.

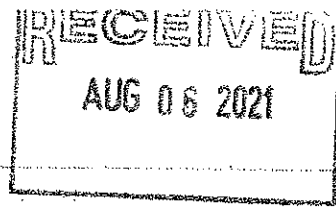
I am unsure of what can be done to scale down this development at this point but I urge you to do what is in your power to have them reconsider their plans.

August 6, 2021 – Address not given

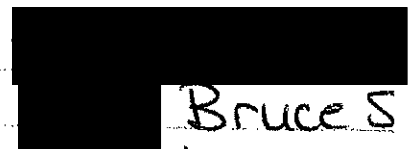
I find myself writing to the planning committee again this week. I must say I am very disappointed in this design it is not keeping with the Cumberland aesthetics. A four store building that holds nothing of the Cumberland character on the outside. I am very proud the way "seeds" "Cumberland brewing" and the pizza place have incorporated their business into the downtown. Disappointed, frustrated are just two of the words I am giving to describe the new "Cumberland vision" our planning committee has taken.

I am hoping you will look further into this design of blending in with the historic feeling of our quaint little village.

Letters



August 6, 2021



Questions for Council - Aug 9 mtg.

1. Will ample time be given by the developer to document and preserve the heritage resources of the building before it is demolished?
2. How will the materials from the demolition of the building be disposed of?
3. Will any components of the original building be incorporated into the new build? ie lobby, restaurant, courtyard
4. Has council or will council consider putting in place
 - temporary heritage protection
 - withholding approvals
 - temporary protection ordersbecause the property is in the HVCC³ in order to allow further consultation by council and developer with the Planning Committee, Heritage Commission, and Cumberland Business Association?
5. Would the developer be agreeable to a Walking Tour of Cumberland, meeting local business owners and visiting the Museum to get a better sense of Village History?¹⁴

6. In Vol. 2 - Heritage Registers the values of the Cumberland Hotel (#60) are described as "an enduring social center for the community." How does the new build incorporate these values?

7. Has/will the developer hold a Neighbour Public Meeting for persons/businesses within a 75 mt. radius of the lot line of the property involved in the development.

8. Will the Village require the developer to submit a security deposit as an assurance that all of council's requests and the developers intentions are met.

To Council

Re: The proposed Eddie

It is disappointing to view the pamphlet proposing the Eddie. It doesn't fit.

The mass of the entrance has no place in our Village core. I understand that they are trying to replicate a mine building but it is not appropriate to Dunsmuir Avenue at this location.

The OCP states that the core heritage character of buildings and streetscapes has remained mostly uniform along Dunsmuir Ave. This is a significant asset and opportunity for the Village. The Eddie detracts and does not add an asset to the Village.

The OCP states creating a Heritage Conservation Area for the Historic Commercial Core is a critical foundation towards achieving appropriate, consistent and sensitive revitalization of this designated area.

This building is not appropriate nor sensitive to the foundation of what is Dunsmuir Ave.

Objectives:

1.1.2(b) development or redevelopment within the HCA respects the history and enhances the heritage character and heritage value of the HVCC

The Eddie fails in enhancing the character and value of the Village Core.

It goes on in (i) for greater certainty...to ensure that new buildings constructed within this HCA are designed and maintained so as not to detract from the overall effect and character of the surrounding structures. All construction on vacant properties located within the boundaries of the Heritage Conservation Area **MUST** be consistent with the Guidelines.

The stepping back of the upper floors is cancelled out by the huge front mass that does not reduce the impact of a tall building rather it emphasizes the huge height and mass of the building and is detracting from the streetscape. This building is **NOT** to be the main focal point of the street but part of the community blending in to the streetscape.

As set out in the Guidelines the form, character and sense of place of the Historic Village Commercial core is reliant on the existence stock of buildings, structures and landscape elements and it is essential that all components work together to provide an integrated and harmonious fashion.

f) The Village requires that new construction conforms to the design and objectives of the HCA.

Sustainable Building

- Building design should include passive heating, lighting and cooling design features. The Units will be extremely hot in the summer as the sun beats down. Where is the external shade? Also wonder where the snow will go from the inside courtyard – will the restaurant have to cart it out to the street?

Parking and amenity Areas

I can't tell how many public bicycle stalls are proposed. I do not see a loading zone for the commercial condos or disability parking spot or guest parking let alone required parking for the commercial enterprises both for their employees and the public.

Claiming that the amenity to the Village is a courtyard that the restaurant/building can lock does not constitute a public amenity. I don't see benches for sitting and watching the street go by. I only see private patios for the restaurant.

I hope that Postmark will keep their original statement of wanting to do what the people want and to actually meet with the people of the Village to end up with a development that works for all. This one gets a fail.

Respectfully submitted

████████████████████

████████ Cumberland Road

Cumberland, BC



August 9, 2021

Thank you for the opportunity to provide comment on the proposed Eddie Building at 2714 Dunsmuir Ave.

Our home supports this project in several areas and raises concerns in others.

As the closest single family home to the proposed project, I look forward to the ways this project will liven up the neighbourhood. In order for this growth to be comfortable it must be responsible.

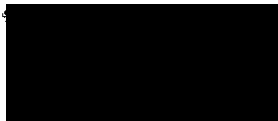
The Development Variance regarding the rear setback is favourable from our point of view. The alley in question has an inordinate amount of commercial traffic. Narrowing the alley at this building site will help commercial transport move its work to Dunsmuir and out of the former coal delivery systems of our alleys.

The reduction in parking space from 30 to 14 will have a profound affect on the residents of Second Street, Dunsmuir & beyond. Without an active parking plan with bylaw enforcement, parking for residents is already problematic, as economic activity on these streets impacts peaceful enjoyment of private residences. Being able to park at home when I return with groceries or goods for my home is already a struggle. For the last several years I have lived with physical impairments and have had to rely on the help of others when I have to park blocks away from my front door. If the Village had a system of Residents Parking on mixed use streets, that would help alleviate some parking pressure. Without some support for single family homes having parking, this variance request is too severe. The Eddie recently shared a post on Instagram, about using an elaborate parking system, Klaus Multiparking, which included a link to a promotional video about the system. The video boasted parking for up to 44. If this project is already planning on compact parking systems, it should respectfully be much closer to the bylaw requirements of this space. The 14 spaces under serves the residences of this proposed building as well as fails to provide visitor parking. This also fails to provide any commercial parking for the 3 proposed spaces. This address is opposed to this portion of the variance request.

The elevation pictures provided in the July 23 letter to neighbouring residences notes the seating area on Dunsmuir Ave, which this house supports. The former business failed to mitigate activity and sound from the Second Street exit which strained neighbourhood relations. Planning on primary activity on Dunsmuir Ave is welcomed.

I look forward to attending as many meetings as possible about this project.

Respectfully,



Second Street



To Council

(kalbert@cumberland.ca)

Re: The proposed Eddie

When I purchased the property at [REDACTED] Dunsmuir Street, and prior to starting restorations to the building, I became aware of the fact that the sewer lines and storm drains for my property were located at the rear of the property next door to my building.

I approached the Village and was told not to worry about it as it was something left over from the colliery days. I am aware of the fact that during the recent upgrading of Dunsmuir Street, proper hook ups were laid, but once again NOT in front of my property, but in front of the adjacent property now being considered for this development.

It appears that a cement wall for the proposed Eddie building will be built right against the stairwell on my property. It would be interesting to know how I can manage any maintenance to that side of my building. In conforming to Village heritage requirements, I installed wood siding which might possibly need some repairs in the future.

It also appears that this cement wall will be built to the height of my building which would block out some of its passive lighting for the upper apartment and would also infringe on the privacy of the tenants in both apartments. The Eddie design proposal indicates large windows for increased passive lighting in its construction. The proposed patio would also impact the privacy of my tenants.

The proposal has also requested reducing the set back from 4.5 to 1.5 meters which appears to have eliminated a loading zone and an area where guests, employees and the public are supposed to park. It would seem that when deliveries are being made, the whole back alley would then be blocked off.

I oppose the Eddie development primarily because of its massive size, the lack of parking which is already a village problem and the above noted reasons.

Respectfully submitted.

[REDACTED]

[REDACTED] Maryport Street

Cumberland, BC

[REDACTED]

2714 Dunsmuir Avenue

E-mail Correspondence received August 9, 2021

From: [REDACTED]

Subject: Eddy Building 2714 Dunsmuir Ave Development Application

Dear Mayor, Council and staff,

I am writing in regards to the development application for the former Cumberland Hotel, 2714 Dunsmuir Ave and the request variances. I want to register my concerns with this development and the impacts such variances would have on downtown Cumberland.

I recognize the need for a diversity of housing in our community and how high density housing meets that need without expanding our footprint, however, I think the variances in this project are problematic. The look of the building should fit with the look and feel of downtown. It currently does not. Additionally, the parking variance is ridiculous.

Parking is already an important issue in this community, with it being difficult to find parking downtown many days. This is before the two condo buildings with parking variances are completed. Another parking variance simply does not make sense. For the able bodied Cumberland is a very walkable community, however for those with disabilities it can be a challenge to get around and the recent upgrades to disabled parking are not enough. Allowing another parking variance will lead to an overall lack of parking for everyone - residents, tourists and customers.

Without robust public transit, ours remains a car dependent community. Developments should be required to have parking. Developments should benefit the community as a whole and not simply wealthy developers. We are in a housing crisis. Our community needs more affordable, dense housing. The decisions made by council, policy and community plans need to reflect this.

Sincerely,

[REDACTED]

On Aug 6, 2021, at 2:18 PM, [REDACTED] wrote:

Subject: Re: Question period

I realize some of these questions may not be for the council to answer. if you're able to please pass this along to the right individuals that would be great. Thank you

> On Aug 6, 2021, at 2:18 PM, [REDACTED] wrote:

>

> Looking at the current postmark group design:

>

> Is there no bylaw restricting the maximum height of buildings being built in downtown core?

> Since no Cumberland downtown core buildings are as high as 5 stories, how was this particular five storey building considered during a proposal, and what led to the decision to allow such big apartment to be designed?

>

> Do you think bringing in 'luxury living' 5 storey condos is what the village needs? How affordable will the apartments be?

>

> Where does the village plan for the residents of the apartment building to be parking their vehicles? If on the street, how will this affect our tourism industry considering the high traffic volume it is already a challenge?

>

> How does this design align with Cumberland's history and heritage, and do you believe allowing a 5 storey 'luxury living' apartment building in Cumberland suits the current character of the 'village' downtown core?

>

> If such building design was to be approved, where do you see Cumberland long term, are more urban style apartments or high buildings like 'The Eddie' going to be considered or are there any considerations being made to preserve our unique downtown core atmosphere which is a highlight of the village for our visitors?

>

> Do you believe this design will be appealing to our visitors and tourists?

>

> Thank you

From: [REDACTED]

Date: August 9, 2021 at 7:44:59 AM PDT

To: [REDACTED]

Subject: Council meeting today

After reading the report i am wondering

1. Where is the site survey that forms part of he heritage alteration permit?

2. Why are the lots not being consolidated? It is not normal to allow a building to straddle two legal descriptions specially when they were created so long ago

3 the OCP states not to use fake heritage yet the whole building has imitation cladding. How green is replacing wood building with concrete building

4 where is the heritage alteration permit for the demolition of the existing Cumberland Hotel? The report has Postmark stating it is a new build which could be on the small half lot

Hu if

5 How is this process engaging citizens in early and ongoing involvement in planning?

Sent from my iPhone

>

From: [REDACTED]
Sent: August 9, 2021 12:27 PM
To: Village of Cumberland <info@cumberland.ca>
Subject: The Eddie proposal

Hello Village of Cumberland

I don't find anything particularly objectionable about the development and I like the nod to the old building with the angled front. The corner facing Dunsmuir will be large and imposing looking, but the setbacks and patio and open spaces all look like they will add to the Cumberland ambiance. The first floor looks like it will flow along from other buildings on Dunsmuir. I looked at the cladding and other features and I think it should look fine. I am sad to see the old Cumberland Hotel be torn down of course, but the new development with a few tweaks will fit in a few years.

That being said, I have concerns on the parking system which seems more suited to storage than parking, and the application to get 15 parking spaces waived. No. Unfortunately the time for wooing new development is long gone and I would like amendments to be made to the plan to add additional parking. Developers can hope people will buy their units and use bikes to travel everywhere in Cumberland but the reality is that a lot of shopping still takes place elsewhere and our bus system is not convenient nor timely. We are a village and want to maintain our ruralish live style but that comes at a cost that should not be borne by the villagers already living here.

Sincerely

[REDACTED]

[REDACTED] Derwent Avenue

[REDACTED]

Comment Sheet

Heritage Alteration Permit Application, 2714 Dunsmuir Avenue

For a new mixed-use commercial and residential building with three commercial spaces on the ground floor and 15 dwelling units above, a parking garage, and small outdoor seating area fronting Dunsmuir Avenue.

Note, this comment sheet will be provided to the Village's planning department.*
Alternatively, comments can be emailed to the Village at planning@cumberland.ca

Name: [REDACTED]

Street Address: [REDACTED] Ulverston Cumberland.

My comments are:

My biggest concern is the height of the Eddie Bldg. I feel it sets a dangerous precedent. The new bldg down the block next to Seeds already creates a huge blank face that blocks off the view of the mountains & it's only 3 stories.

Some of the design features of Eddie seem to make an attempt to be less imposing - ie the setbacks etc.

The diagrams showed some of it to go right to the edge of the alleyway. Is this correct? There should be a little more easement.

Much as I know 3 stories will affect their financial success, I believe that 4 stories is not a good thing for Cumberland.

* Personal information in submissions is collected by the Village pursuant to sections 26(a) and (c) of the *Freedom of Information and Protection Privacy Act* for the purpose of gaining public feedback and representation on the Heritage Alteration Permit application. Names will be redacted (blacked out) but submissions will become part of the public record.

August 7, 2021

Village of Cumberland Planning Department:

RE: Response to Heritage Alteration Permit Application 2714 Dunsmuir Ave. Cumberland BC

I appreciate, that we, in Cumberland had the opportunity to attend the open house regarding the above permit application. I would like to complement Postmark Development on the presentation. In my opinion, they have done a good job researching the needs of our village and overall, I think the proposed build will be an asset to our community. It is my hope that senior staff and council continues to work with them for a successful build. The last thing I would like to see is another **vacant** building sitting empty on our beautiful main street that is tied up in red tape! I do understand that this building is a big change, but change is inevitable for our Village and we must begin to think innovatively and critically about this inevitable process. I think back to where we were 20 years ago. I realize that some changes were not as positive as we would have hoped for, but I am happy we have moved forward and hope we continue to do so toward Village successes.

I do however have several concerns. Based on my 18+ years experience as a maintenance team member with Island Health I feel several matters are worth mentioning. When I asked about the heating and cooling systems, there was no consideration with cooling. After recent events (heat dome of 2021), we at Island Health learned that many of our buildings, including our own Cumberland Lodge and our two new hospital buildings were not built for extreme heat events. In my opinion, it is very short sighted to build the Eddie without thinking of inevitable climate change. I do not think we would like to see air conditioning units hanging out of a brand new building and I do not believe natural airflow will be sufficient to make it a comfortable environment when we experience future heat waves. I also have to think about our winter months. Is there any consideration on snow removal should we get a deep winter.

I was also disappointed that there will be no green initiatives such as solar power, rainwater catchment and other climate change considerations.

I also have to question the maximum use of the back alley. Is there sufficient room for deliveries to the businesses such as food and beverage deliveries or stock for retail sales? There also seems limited room for waste removal.

In conclusion, I appreciate the intent of the Postmark Group naming the project after what sounds to be a very lovely man. However, do we really want the name of what could be an iconic landmark on our main street named after a wealthy white man from another province? I think the group captured the essence of architecture in the village and struggled to make a four story build fit into the existing buildings but failed to understand the historic culture and work ethic of our village.

Again, I hope this project is a success for Postmark and goes forward and is constructed and completed in a timely manner.

Regards,

██████████

██████████ Grizeadale Ave

Cumberland B.C.

████████████████████

Comments Received after 2 pm August 9, 2021

HAP-02-2021

Dear respected members of council and Mayor,

I am writing in regards to 2814 Dunsmuir Ave DVP that is on the agenda Monday August 9th 2021. The sign for this DVP has just been put up this past week. We live two blocks away, and just noticed it. I am opposed to allowing any variances approvals to this developer/land owner. They have the opportunity of working with a blank slate on lot B and aren't really providing a lot of details of what they are putting on the lot. It's indicated: "The applicant is proposing to move a dwelling onto proposed lot B and renovate the structure to increase the size of the building footprint (Attachment 1 – Proposed Plan of Subdivision)." Move what dwelling into that site? Is it two levels? Three levels? Is two units the way to rent out? One level family home? How much parking should be provided? Are they moving the house from lot A over to lot B, raising it for two dwellings, then building another two story rental unit on lot A requiring MORE variances? Ask currently lot A has NO parking at all. Today there is a large camper on the side property and two cars parked on the grass.

The drawing is what is being proposed on lot B, next to the alley on sixth street. But then the choices that council has been given is:

1. THAT Council approve the application (2021-08-DV) for a Development Variance Permit on the property described as Lot 1, Block 27, DL 21, Plan VIP522C (2814 Dunsmuir Avenue) and advise staff to issue the permit.
2. THAT Council deny the application (2021-08-DV) for a Development Variance Permit on the property described as Lot 1, Block 27, DL 21, Plan VIP522C (2814 Dunsmuir Avenue).

Is Lot 1, Block 27, DL 21, Plan VIP522C the Lot A or the Lot B?

I don't feel that a lot of information is being shared here. If you have the answers, I'd love to hear them. But as it stands, this is really clear to me what is being proposed.

When working with a blank slate, work within the guidelines. You bought either the empty lot B , the house on Lot A or both and should have done your research first. You should work within the current restrictions set in place to build something that suits the neighbourhood. It's no wonder long term residents keep moving away (as the neighbours next door to this VERY site) no one wants this next door to them. No one wants to go from one household to now potentially 4 households next to them.

 Maryport Ave

Please could we make sure that major developments like the Eddie don't come to council in mid summer?

Thanks

Ulverston Avenue

My biggest concern is the height of the Eddie Bldg. I feel it sets a dangerous precedent. The new bldg. down the block next to Seeds already creates a huge blank face that blocks off the view of the mountains and its only 3 stories.

Some of the design features of Eddie seem to make an attempt to be less imposing, i.e., the setbacks etc.

The diagrams showed some of it to go right to the edge of the alleyway. Is this correct? There should be a little more easement.

Much as I know 3 stories will affect their financial success. I believe that 4 stories is not a good thing for Cumberland.

Re: The Eddie- 2714 Dunsmuir Ave

First off, I would like to thank you all for your time and consideration on the contentious matter of The Eddie and the Cumberland Hotel site. Over the last 5 years that I have lived in the village I have seen the incremental change and outside influence as it has slowly eroded its character and feel.

One of the main reasons that I fell in love with Cumberland, a village in the mountains, was that it was exactly that, a small village unobtrusively integrated into the mountains. The proposed plan for The Eddie is the most intrusive, out-of-touch proposal I have seen to date.

The size and prominence of the building are completely out of character for our village. I understand the need for more affordable housing in the Comox Valley and for Cumberland in particular, however I do not agree that it should come at the cost of the village losing the charm and character that everyone loves it for. The proposed plan seems to tug at the heartstrings of the village promising a restaurant, a clinic, a bike shop, and affordable housing- at the cost of ruining the aesthetics of the main street, overloading the already overtaxed parking situation, and neutering the heritage and culture that makes Cumberland so wonderful. The proposed height and mass of the building are the Cumberland equivalent of an aseptic downtown skyscraper.

I hope that the Council and Mayor consider the effect that opening the door to out-of-touch outside development will do to the Village. I do not have the answers but would be happy to see a reduction in the height and footprint of the building- one less floor means 5 less parking spaces to get a variance for.

Thank you for your consideration.

██████████
Homeowner, business owner.

Hello Mayor Baird and Council,

Please accept the following concerns regarding The Eddie development:

11.2.2

A. Parking as proposed by the developer only meets the minimum residential requirement. I am very concerned about the requested variance for commercial requirements. Parking is already an issue for residents and visitors. From an economic development perspective, having appropriate parking available for people looking to access the shops, services and restaurants of our downtown core is very important.

F. Scale of new construction does not respect nor complement the adjacent structures. The proposed 4-story structure is not consistent with surrounding buildings or the original structure on the property. The height, mass and aesthetic of the proposed building does not compliment or fit with the downtown core.

G. The original proportions shall be retained. The proposed structure is not comparable at all to the existing building in design nor in the sheer mass/size. Also, there is nothing 'Tudor' about the style as the developer suggests.

The terms 'historic' and 'vibrant' is used through our OCP. However, from the visual renderings and the wording in The Eddie development proposal there is nothing vibrant or historic about the aesthetic of the new building.

I am concerned that if historic variances and parking variances are again permitted in this development, as they were for the condos at Dunsmuir/3rd, future developments will all be granted the variances as well.

What do we want for our downtown core in the longer term? What is authentic for Cumberland? I feel that our current economic development strategy and OCP do not support the requested variances.

Thank you for your time.

Regards,

██████████

Planning and development,

In regards to the new building slated for the location where the Cumberland Hotel is located.

I believe keeping the building to the height of two stories, off street parking to be provided for each unit and attractive or interesting architecture is important for the well being of Cumberland and Cumberlands future.

Thank you, ██████████